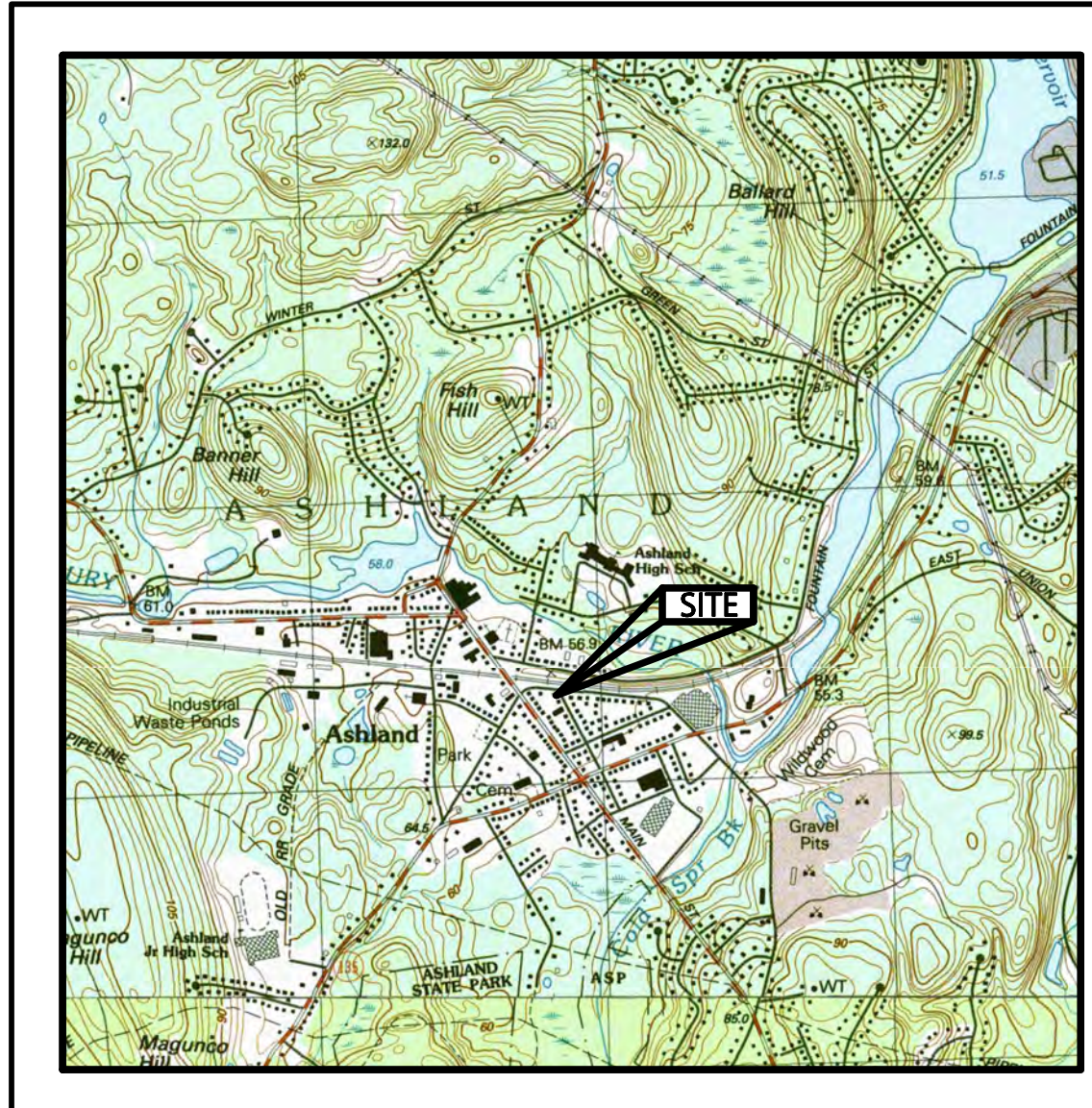


PROPOSED MIXED USE DEVELOPMENT

9-49 Homer Avenue
 (Tax Map 14, Lots 352-354)
 Ashland, Massachusetts 01721

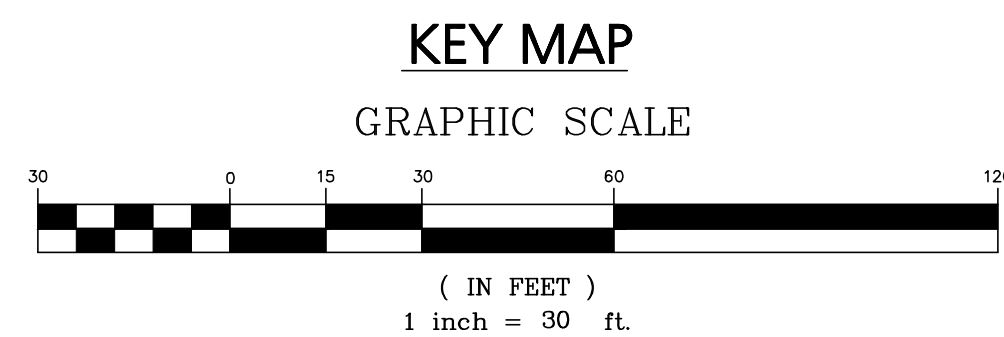
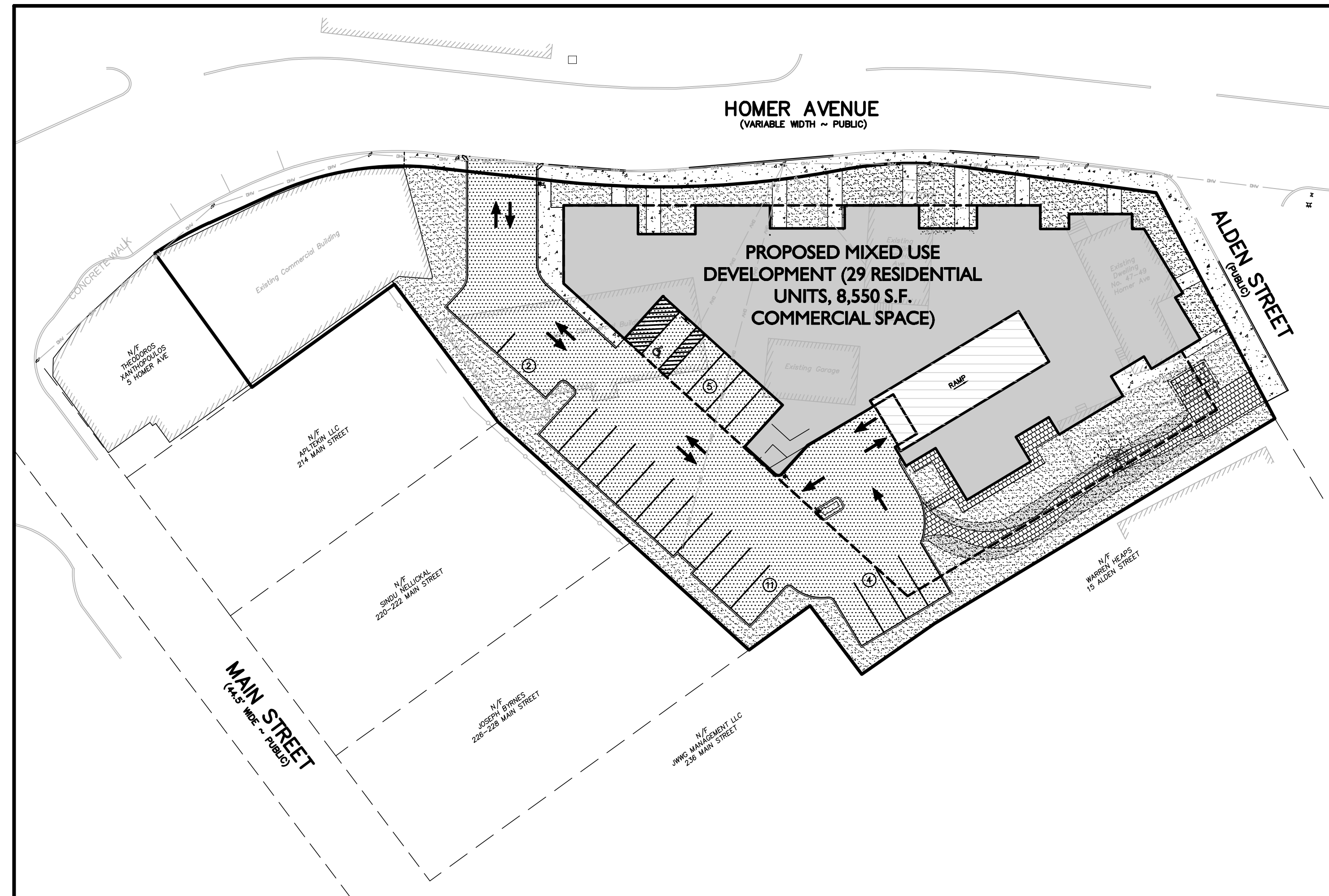


LOCUS MAP
 SCALE: 1:25,000

OWNER/APPLICANT:
 9-49 HOMER AVENUE, LLC
 11 PLACID ROAD
 NEWTON, MA 02549

CIVIL ENGINEER:
 ENGINEERING ALLIANCE, INC.
 194 CENTRAL STREET
 SAUGUS, MA 01906
 (781) 231-1349

ARCHITECT:
 BOURQUE DESIGN
 9 MORTON STREET
 WALTHAM, MA 02543
 (781) 296-6654



PREPARED BY:

Engineering Alliance, Inc.
 Civil Engineering & Land Planning Consultants
 194 Central Street
 Saugus, MA 01906
 Tel: (781) 231-1349
 Fax: (781) 417-0020

KEY MAP
 GRAPHIC SCALE

PREPARED FOR:
 9-49 Homer Avenue, LLC
 11 Placid Road
 Newton, MA 02549

SHEET NUMBER AND TITLE

- C-0 COVER SHEET
- C-1 EXISTING CONDITIONS PLAN
- C-2 EROSION CONTROL & DEMOLITION PLAN
- C-3 SITE LAYOUT PLAN
- C-4 GRADING, DRAINAGE & UTILITIES PLAN
- D-1 CONSTRUCTION DETAILS I
- D-2 CONSTRUCTION DETAILS II

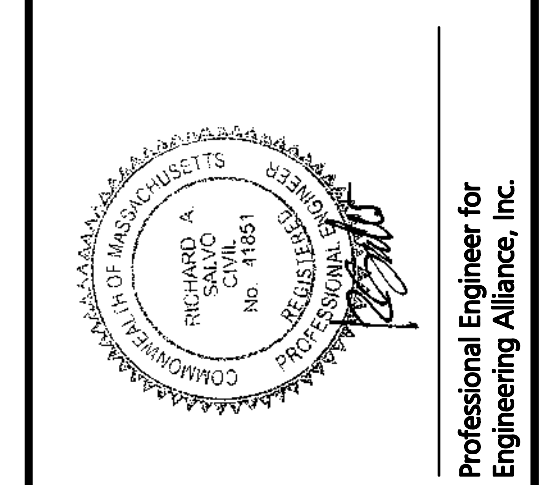
DATE	DESCRIPTION OF REVISION
7/28/23	MODIFIED PER PEER REVIEW COMMENTS
10/11/22	UPDATED BUILDING FOOTPRINT

PREPARED BY:

Engineering Alliance, Inc.
 Civil Engineering & Land Planning Consultants
 194 Central Street
 Saugus, MA 01906
 Tel: (781) 231-1349
 Fax: (781) 417-0020

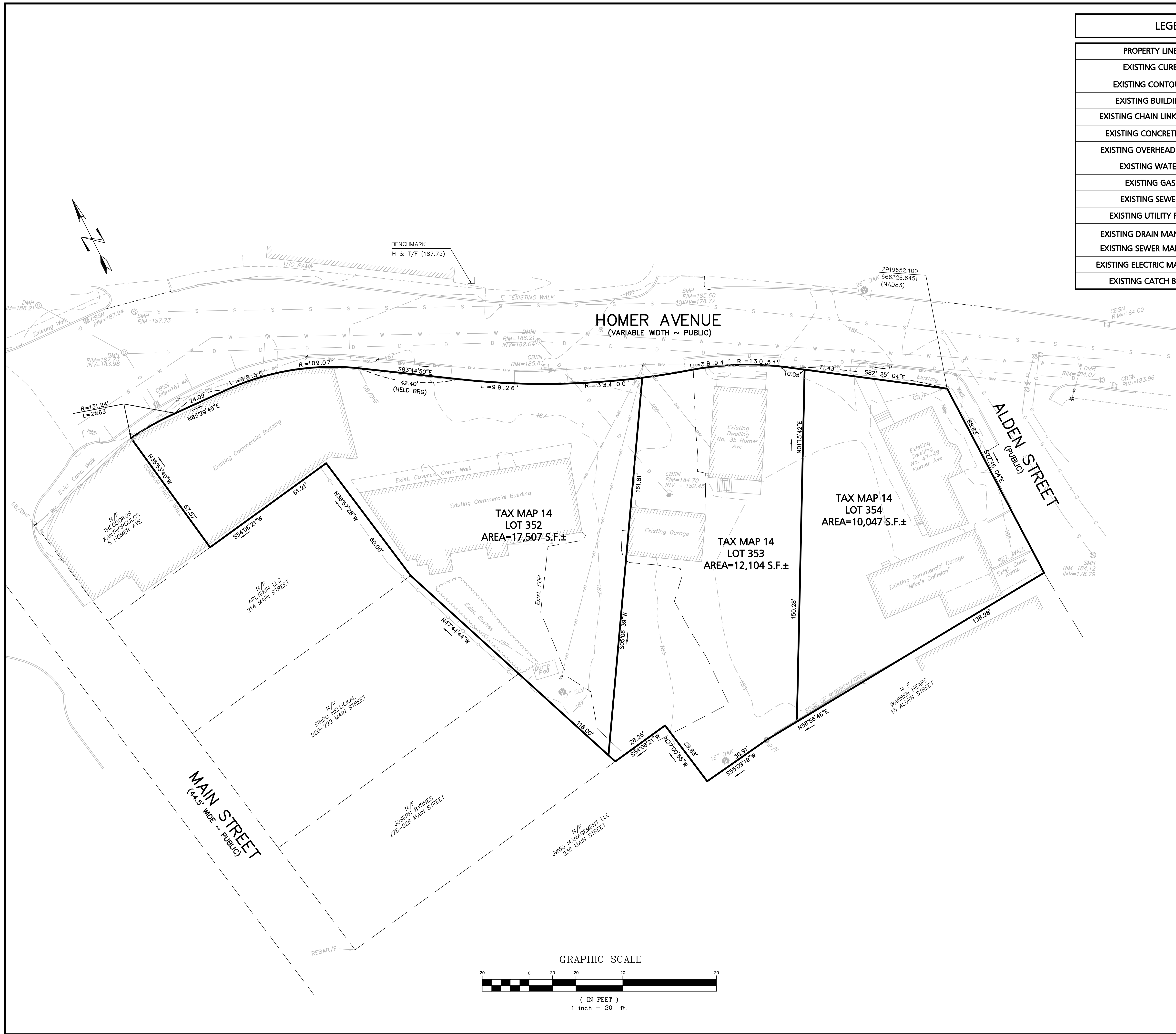
PROJECT:
 Proposed Site Plan
 9-49 Homer Avenue
 (Tax Map 14 Lots 352-354)
 Ashland, Massachusetts

DATE: June 17, 2022
 DWG FILE NAME: 21-58508.dwg
 SCALE: AS NOTED
 CHECKED BY: Richard A. Salvo, P.E.
 DESIGN BY: Eric Bradshaw, P.E.



APPLICANT:
 9-49 Homer Avenue, LLC
 11 Placid Road
 Newton, MA 02549

DWG. NO. **C-0**
 DRAWING TITLE:
Cover Sheet



LEGEND - EXISTING CONDITIONS PLAN	
PROPERTY LINE	—————
EXISTING CURB	—————
EXISTING CONTOUR	----- 186 -----
EXISTING BUILDING	//////
EXISTING CHAIN LINK FENCE	—————
EXISTING CONCRETE WALL	▨▨▨▨
EXISTING OVERHEAD WIRES	----- DW -----
EXISTING WATER	—— W —— W ——
EXISTING GAS	—— G —— G ——
EXISTING SEWER	—— S —— S ——
EXISTING UTILITY POLE	⊕
EXISTING DRAIN MANHOLE	⊙
EXISTING SEWER MANHOLE	⊙
EXISTING ELECTRIC MANHOLE	⊙
EXISTING CATCH BASIN	⊕

GENERAL NOTES:

OWNER OF RECORD: WJK HOMER, LLC
10 WAUSHAKUM, LLC
DEED REFERENCE: BOOK 72258 PG 337
TAX MAP REFERENCE: MAP 14 LOTS 352-354
DATUM REFERENCE: NAVD 1988
ACCORDING TO THE F.E.M.A. MAP FOR SOUTHERN MIDDLESEX COUNTY, MAP No. 2501700514F DATED JULY 7, 2014, THE PARCELS SHOWN ON THIS PLAN LIES WITHIN A ZONE "X" (OUTSIDE OF 0.02% CHANGE).
EXISTING CONDITIONS INFORMATION SHOWN ON THIS PLAN WAS COMPILED FROM AN ACTUAL FIELD SURVEYS PERFORMED BY HAWK CONSULTING INC ON FEBRUARY 19, 2019 AND MARCH 22, 2021.

DATE	DESCRIPTION OF REVISION
7/28/23	MODIFIED PER PEER REVIEW COMMENTS

Prepared By:

Engineering Alliance, Inc.
Civil Engineering & Land Planning Consultants
1950 Lafayette Road
Portsmouth, NH 03801
Tel: (603) 5107100
Fax: (603) 6107101

PROJECT:

Proposed Site Plan
9-49 Homer Avenue
(Tax Map 14 Lots 352-354)
Ashland, Massachusetts

DATE: June 17, 2022
DWG FILE NAME: 21-58508.dwg
SCALE: AS NOTED
DESIGN BY: Eric Bradnase, P.E.
CHECKED BY: Richard A. Salvo, P.E.

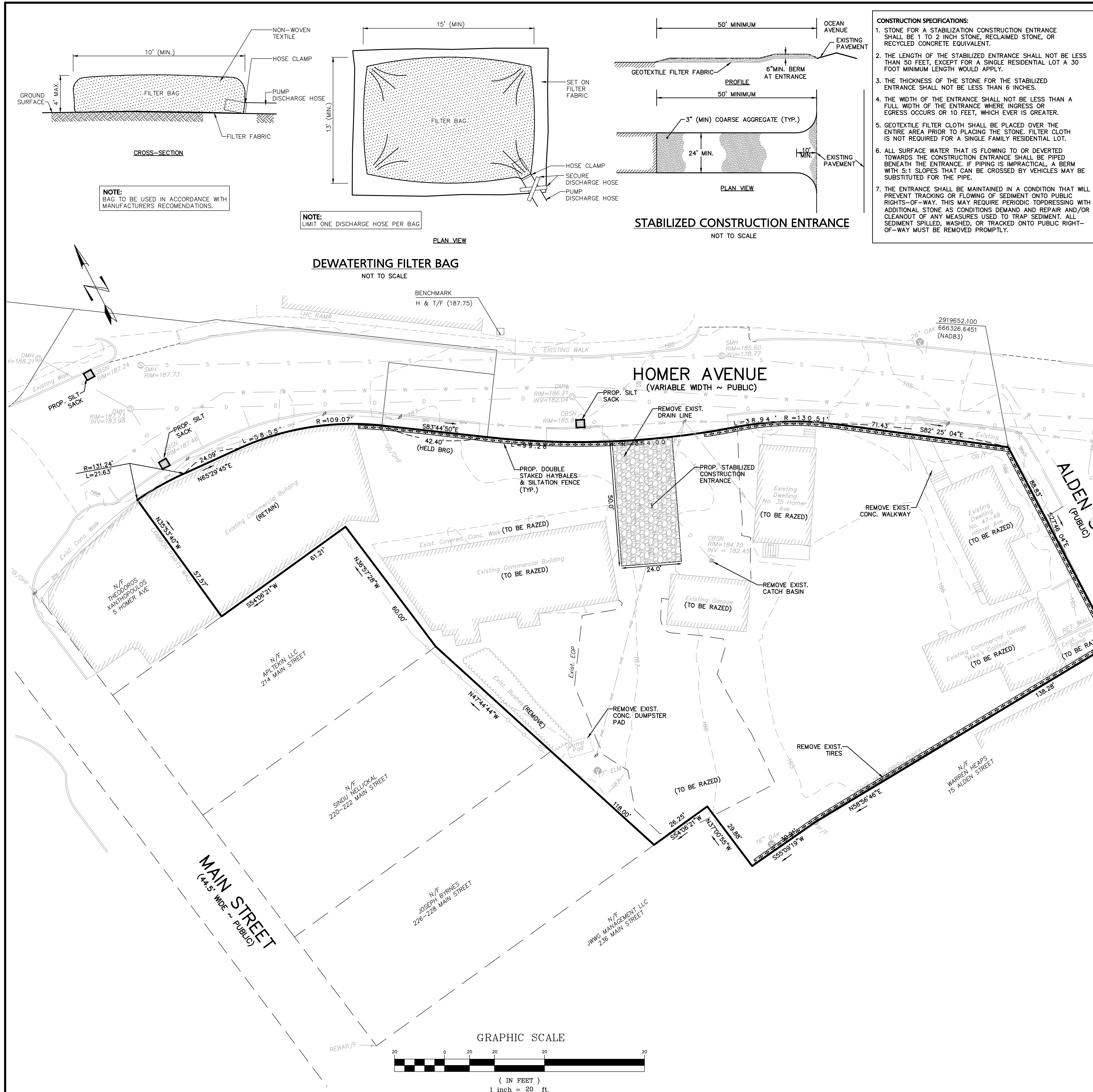
Professional Engineer for
Engineering Alliance, Inc.

APPLICANT:

9-49 Homer Avenue, LLC
11 Pileold Road
Newton, MA 02459

DRAWING TITLE:
Existing Conditions
Plan

DWG. NO.:
C-1



CONSTRUCTION SPECIFICATIONS:

1. STONE FOR A STABILIZATION CONSTRUCTION ENTRANCE SHALL BE 1 TO 2 INCH STONE, RECLAIMED STONE, OR RECYCLED CONCRETE EQUIVALENT.
2. THE LENGTH OF THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 50 FEET, EXCEPT FOR A SINGLE RESIDENTIAL LOT A 30 FOOT MINIMUM LENGTH WOULD APPLY.
3. THE THICKNESS OF THE STONE FOR THE STABILIZED ENTRANCE SHALL NOT BE LESS THAN 6 INCHES.
4. THE WIDTH OF THE ENTRANCE SHALL NOT BE LESS THAN A FULL WIDTH OF THE ENTRANCE WHERE INGRESS OR EGRESS OCCURS OR 10 FEET, WHICH EVER IS GREATER.
5. GEOTEXTILE FILTER CLOTH SHALL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING THE STONE. FILTER CLOTH IS NOT REQUIRED FOR A SINGLE FAMILY RESIDENTIAL LOT.
6. ALL SURFACE WATER THAT IS FLOWING TO OR DEVERTED TOWARDS THE CONSTRUCTION ENTRANCE SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A BERM WITH 5:1 SLOPES THAT CAN BE CROSSED BY VEHICLES MAY BE SUBSTITUTED FOR THE PIPE.
7. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOPDRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, WASHED, OR TRACKED ONTO PUBLIC RIGHT-OF-WAY MUST BE REMOVED PROMPTLY.

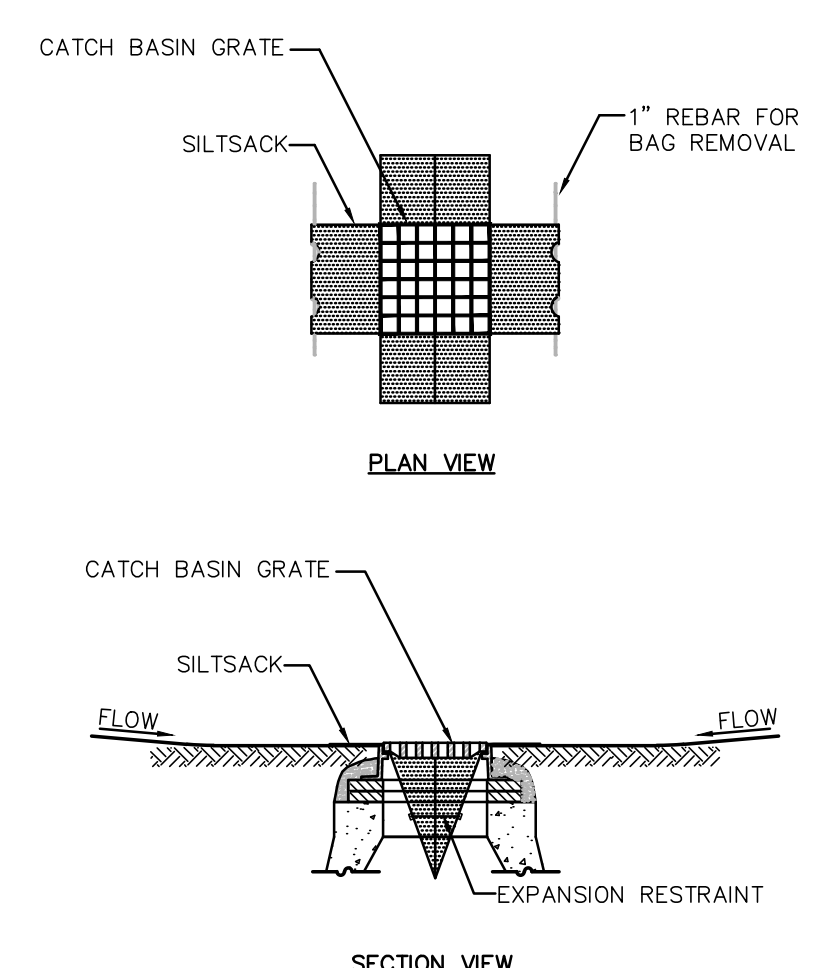
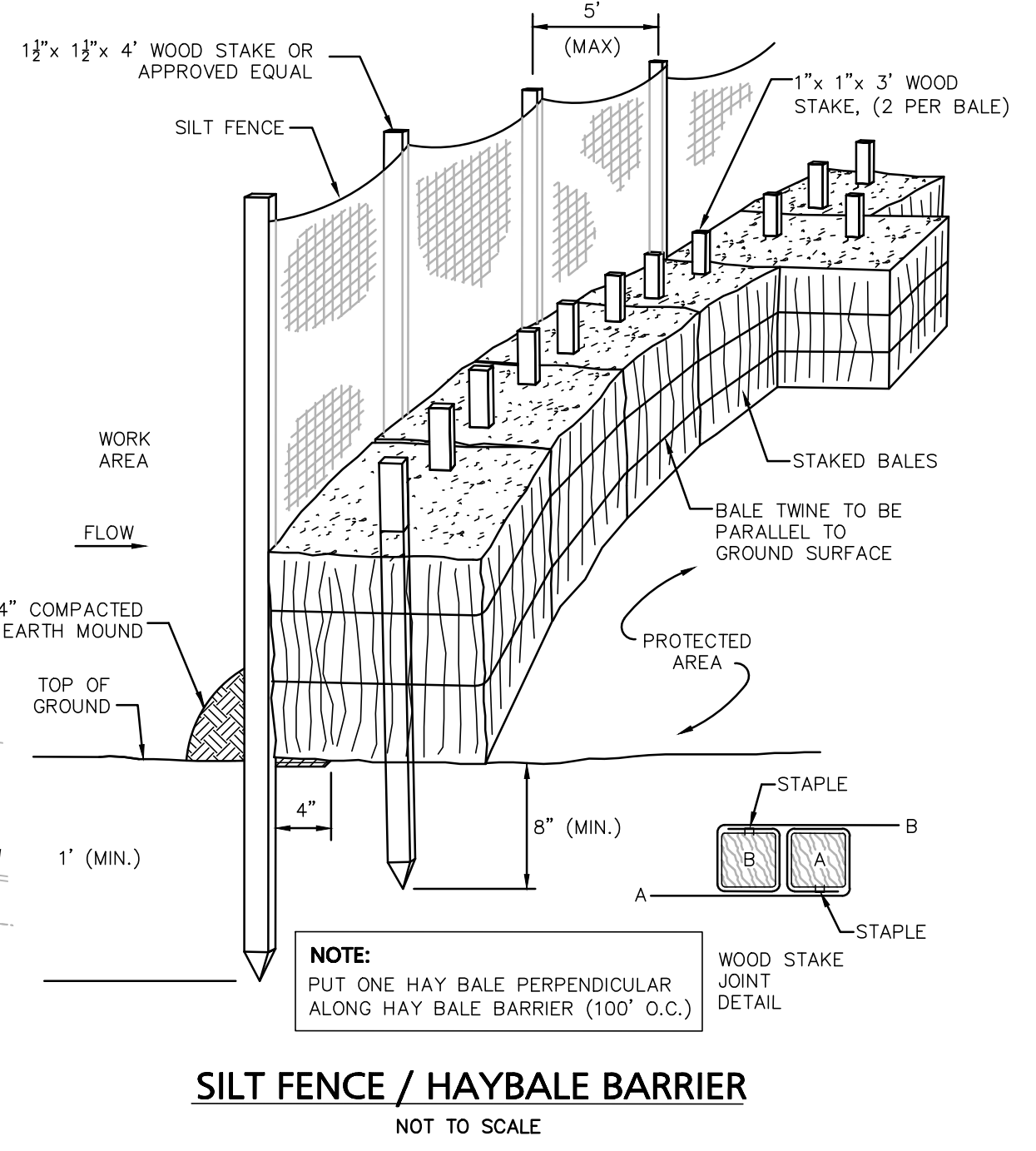
LEGEND - EROSION CONTROL PLAN

PROPERTY LINE

PROPOSED HAYBALE & SILTATION FENCE

DEMOLITION NOTES:

1. EXISTING BUILDING STRUCTURES AND ALL ASSOCIATED APPURTENANCES ARE TO BE REMOVED OR DEMOLISHED UNLESS OTHERWISE NOTED. THE CONTRACTOR IS RESPONSIBLE FOR THE STOCKPILING AND REMOVAL OF ALL MATERIAL DURING DEMOLITION PROCESSES.
2. EXISTING UTILITIES SHOWN ON THIS PLAN WERE DETERMINED FROM VISIBLE STRUCTURES AND PLANS OF RECORD WHERE AVAILABLE. THE CONTRACTOR IS RESPONSIBLE FOR THE CUTTING, CAPPING, AND REMOVAL OF ALL EXISTING UTILITIES ASSOCIATED WITH EXISTING INFRASTRUCTURE. ANY ADDITIONAL UTILITY INFORMATION DIFFERING FROM THAT SHOWN ON THIS PLAN SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.



NOTES:

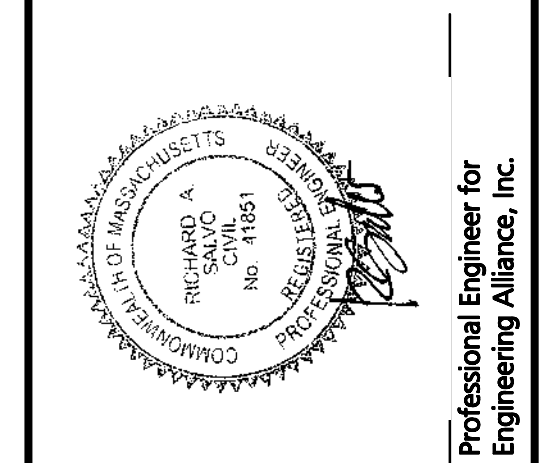
1. INSTALL SILTSACK IN ALL CATCH BASINS WHERE INDICATED ON THE PLAN BEFORE COMMENCING WORK OR IN PAVED AREAS AFTER BINDER COURSE IS PLACED AND HAY BALES HAVE BEEN REMOVED.
2. GRATE TO BE PLACED OVER SILTSACK.
3. SILTSACK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS AND CLEANING OR REPLACEMENT SHALL BE PERFORMED PROMPTLY AS NEEDED. MAINTAIN UNTIL UPSTREAM AREAS HAVE BEEN PERMANENTLY STABILIZED.

NO.	DATE	DESCRIPTION OF REVISION

Engineering Alliance, Inc.
Civil Engineering & Land Planning Consultants
194 Central Street
Saugus, MA 01906
Tel: (781) 231-1349
Fax: (781) 417-0020

Proposed Site Plan
9-49 Homer Avenue
(Tax Map 14 Lots 352-354)
Ashland, Massachusetts

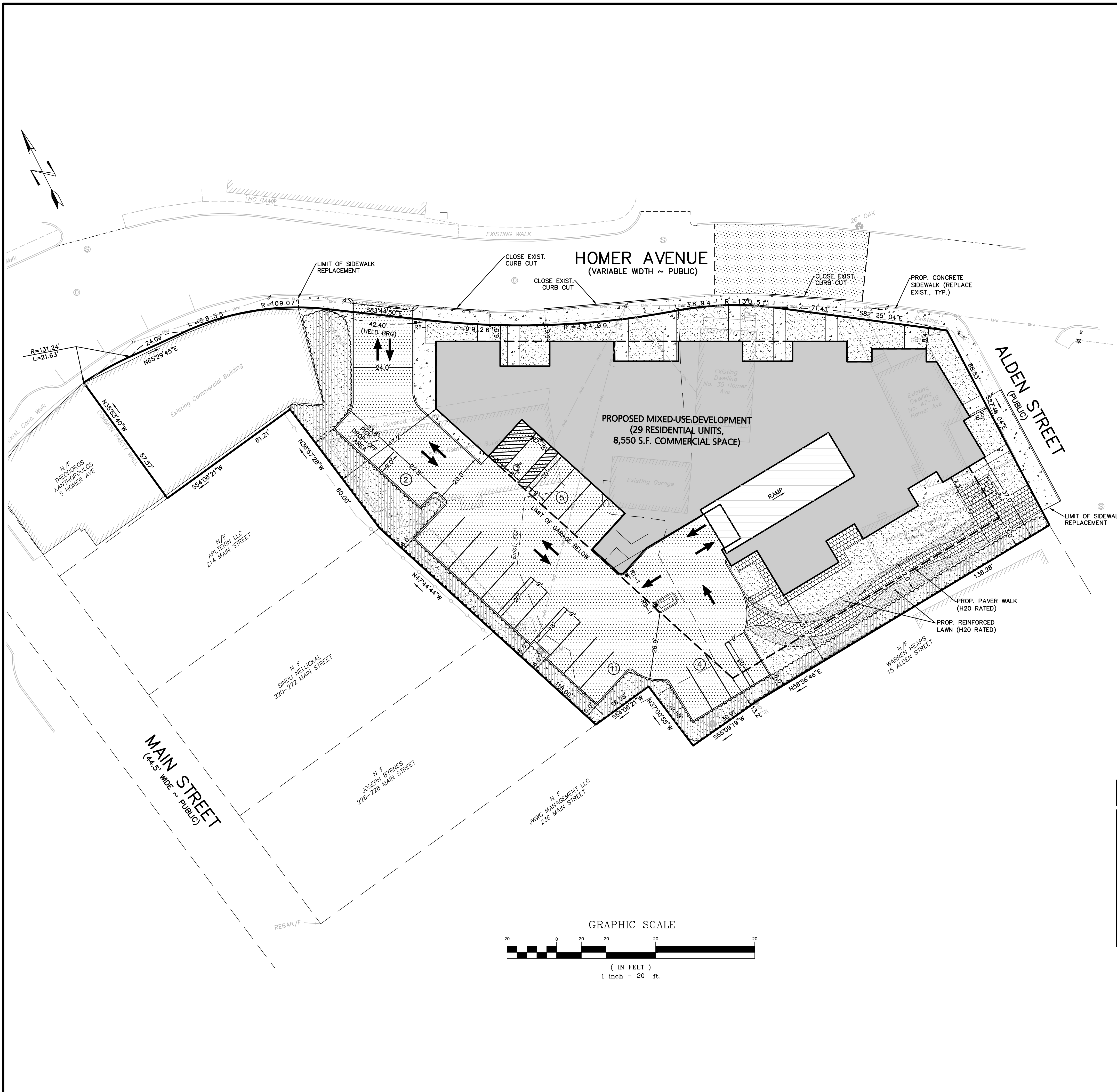
DATE: June 17, 2022
DWG FILE NAME: 21-58508.dwg
SCALE: AS NOTED
DESIGN BY: Eric Bradshaw, P.E.
CHECKED BY: Richard A. Salvo, P.E.



APPLICANT: 9-49 Homer Avenue, LLC
11 Pileold Road
Newton, MA 02459

DRAWING TITLE: Erosion Control & Demolition Plan

DWG. NO.: C-2



LEGEND - SITE LAYOUT PLAN	
PROPERTY LINE	—————
EXISTING BUILDING	▨▨▨▨▨▨▨▨▨▨
EXISTING EDGE OF PAVEMENT	- - - - -
EXISTING CURB	=====
PROPOSED BUILDING	▬▬▬▬▬▬▬▬▬▬
PROPOSED PARKING COUNT	③
PROPOSED TRAFFIC ARROW	➔
PROPOSED ACCESSIBLE PARKING SPACE	♿
PROPOSED LANDSCAPED AREA	▨▨▨▨▨▨▨▨▨▨
PROPOSED CONCRETE SIDEWALK	▬▬▬▬▬▬▬▬▬▬
PROPOSED SNOW STORAGE AREA	⊖⊖⊖⊖⊖⊖⊖⊖

LAND USAGE TABLE ASHLAND DOWNTOWN DISTRICT (ADD)			
ITEM	REQUIRED	PROVIDED	EXEMPT REQUESTED
MINIMUM LOT AREA (S.F.)	8,000 S.F.	39,658 S.F.	NO
MINIMUM FRONTAGE	50 FT	HOMER: 353.31 FT ALDEN: 88.83 FT	NO
MINIMUM FRONT YARD SETBACK	8 FT	HOMER: 6.5 FT ALDEN: 8.0 FT	YES
MAXIMUM FRONT YARD SETBACK	15 FT		
MINIMUM SIDE YARD SETBACK	-	N/A	NO
MINIMUM REAR YARD SETBACK	12 FT	13.0 FT	NO
MAXIMUM NUMBER OF STORIES	3 (4) ¹	3	NO
MAXIMUM HEIGHT (FT)	35 (45) ²	35 FT	NO
MAXIMUM FAR	1.5	1.03 ³	NO

NOTE:
 1. AN ADDITIONAL FLOOR MAY BE ADDED IF AGREED TO BY PLANNING BOARD.
 2. 45' BUILDING HEIGHT IS ALLOWED UPON ISSUANCE OF SPECIAL PERMIT.
 3. FAR:
 FIRST FLOOR: 11,996 S.F.
 SECOND FLOOR: 12,534 S.F.
 THIRD FLOOR: 12,404 S.F.
 TOTAL: 36,934 S.F.
 9-11 HOMER AVE. 3,980 S.F.
 TOTAL: 40,914 S.F.
 40,914 / 39,658 = 1.03

PARKING CALCULATIONS		
COMPONENT	REQUIRED	PROPOSED
MULTI-FAMILY DWELLING RESIDENTIAL (29 UNITS)	58 SPACES (2 SPACES PER UNIT) 29 UNITS x 2 spaces = 58 Spaces	22 SPACES (At Grade Spaces) 41 SPACES (Below Grade Spaces)
OFFICE, STORES	48 SPACES (1 SPACE PER 180 S.F.) 8,550 s.f./180 s.f. = 47.5 Spaces	3 SPACE (Accessible Parking)
TOTAL	106 SPACES x 56.25% = 60 SPACES*	66 SPACES

*PER SECTION 8.5.13 OF THE TOWN OF ASHLAND ZONING BYLAW: "WHERE THERE IS A MIXTURE OF LAND USES ON ONE LOT, THE NUMBER OF PARKING SPACES REQUIRED SHALL BE ONLY 75% OF THOSE STIPULATED ABOVE, THAT IS, WHERE THIS IS A MIXTURE OF USES THE PARKING REQUIREMENT WILL BE 56.25% (75% TIMES 75%)."
 NOTE:
 1. TOTAL NUMBER OF PARKING SPACES IN DEVELOPMENT: 65 SPACES
 MINIMUM NUMBER OF ACCESSIBLE PARKING SPACES REQUIRED FOR 51-75 SPACES: 3 SPACES
 2. STANDARD PARKING SPACES ARE 9'x20'
 3. REFER TO ARCHITECTURAL PLANS FOR LAYOUT OF SUBSURFACE GARAGE PARKING.

SIGN TABLE					
REGULATORY DESCRIPTION	SIGN	SIZE	MOUNTING HEIGHT (BASED TO BOTTOM OF SIGN)	DESCRIPTION	REFLECTORIZED
R7-BV		12" x 20"	7' - 0"	BLUE ON WHITE	YES
R1-1		24" x 24"	7' - 0"	WHITE ON RED	YES
RS-1		24" x 24"	7' - 0"	WHITE ON RED	YES

PREPARED BY: **Engineering Alliance, Inc.**
 Civil Engineering & Land Planning Consultants
 194 Central Street
 Portsmouth, NH 03801
 Tel: (603) 610-7100
 Fax: (603) 417-0020

MODIFIED PER PEER REVIEW COMMENTS
 UPDATED BUILDING FOOTPRINT
 DESCRIPTION OF REVISION

PROJECT: **Proposed Site Plan
9-49 Homer Avenue
(Tax Map 14 Lots 352-354)
Ashland, Massachusetts**

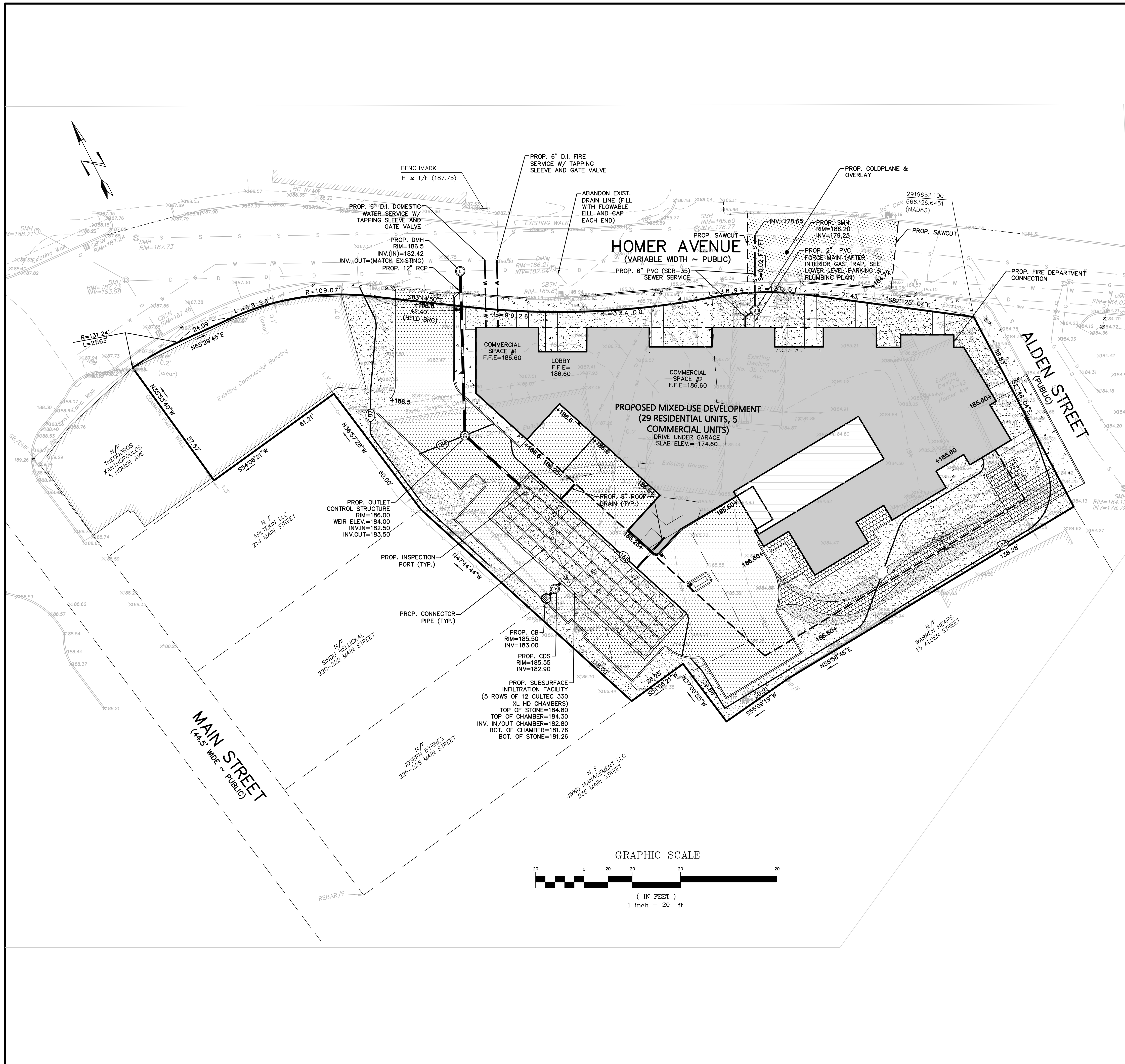
DATE: June 17, 2022
 DWG FILE NAME: 21-58508.dwg
 CHECKED BY: Richard A. Salvo, P.E.

PROJECT # 21-58508
 SCALE: AS NOTED
 DESIGN BY: Eric Bradanese, P.E.

APPLICANT: **9-49 Homer Avenue, LLC**
 11 Placid Road
 Newton, MA 02459

DWG. NO. **C-3**

DRAWING TITLE: **Site Layout**



LEGEND - GRADING, DRAINAGE, & UTILITY

PROPERTY LINE	— — — — —
PROPOSED BUILDING	▬▬▬▬▬▬▬
PROPOSED DRAIN LINE	— — — — —
PROPOSED CDS UNIT W/ GRATE	⊙
PROPOSED SEWER LINE	— S — S —
PROPOSED SEWER CLEANOUT	●
PROPOSED GAS TRAP	⊙
PROPOSED WATER LINE	— W — W —
PROPOSED GATE VALVE	⊗
PROPOSED WATER SHUT OFF	WSO
PROPOSED SPOT SHOT	+81.5

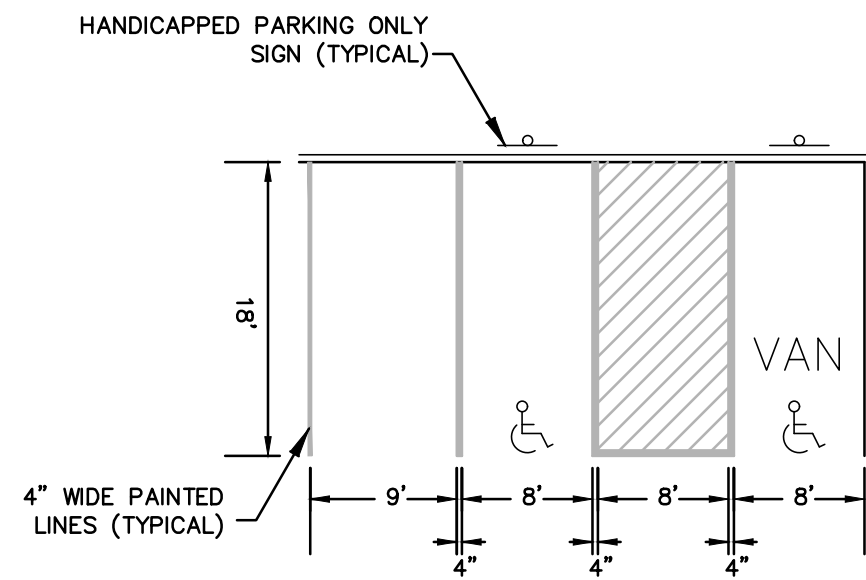
GENERAL UTILITY NOTES:

1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AND STRUCTURES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF VARIOUS UTILITY COMPANIES AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THIS INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE LOCATION OF ALL UNDERGROUND UTILITIES AND STRUCTURES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANY, ANY GOVERNING PERMITTING AUTHORITY, AND "DISSAFE" AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION WORK TO REQUEST EXACT FIELD LOCATION OF UTILITIES. UTILITIES BOTH ON-SITE AND WITHIN THE RIGHT-OF-WAY SHALL BE MARKED OUT PRIOR TO CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY UTILITIES INTERFERING WITH THE PROPOSED CONSTRUCTION AND APPROPRIATE REMEDIAL ACTION TAKEN BEFORE PROCEEDING WITH THE WORK. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLAN.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING ALL CONTROL POINTS AND BENCHMARKS NECESSARY FOR THE WORK.
3. THE CONTRACTOR SHALL EXCAVATE TEST PITS PRIOR TO COMMENCING WORK TO DETERMINE THE EXACT LOCATION OF WATER AND GAS SERVICES.
4. ALL PROPOSED WORK SHALL BE PERFORMED IN FULL COMPLIANCE WITH THE TOWN OF ASHLAND, AND IS SUBJECT TO QUALITY CONTROL TESTING AT THE DISCRETION OF THE ENGINEERING DEPT. AT THE EXPENSE OF THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE TOWN OF ASHLAND D.P.W. PRIOR TO THE COMMENCEMENT OF ANY UTILITY WORK. ALL WATER WORK SHALL BE IN COMPLIANCE WITH ASHLAND CHAPTER 334 AND ALL SEWER WORK SHALL BE IN COMPLIANCE WITH ASHLAND CHAPTER 326.
5. ALL UTILITY WORK WITHIN ANY RIGHT-OF-WAY SHALL BE PERFORMED BY A CONTRACTOR LICENSED BY THE DPW & OBTAIN A PERMIT FOR SUCH WORK FROM THE DPW.
6. ANY CHANGE IN THE FIELD CONDITIONS SHOULD BE REPORTED TO THE ENGINEER TO ENSURE THAT ANY MODIFICATIONS TO THE ORIGINAL DESIGN CONFORM TO STANDARD ENGINEERING AND CONSTRUCTION PRACTICES AND ADEQUATE TO SERVE THE PROJECT'S NEEDS AND COMPLY WITH APPLICABLE STANDARDS AND REGULATIONS.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING ALL UTILITIES AS SHOWN ON THESE PLANS IN ACCORDANCE WITH THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY CONSTRUCTION SHALL CONFORM TO THE APPROPRIATE UTILITY COMPANY STANDARDS FOR CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING SPECIFICATIONS OF MATERIALS AND INSTALLATION PROCEDURES AND INSTALL IN ACCORDANCE WITH THESE REGULATIONS.
8. THE CONTRACTOR IS RESPONSIBLE TO CONTACT AND DETERMINE, COORDINATE AND SCHEDULE ALL NECESSARY INSPECTIONS AND MONITORING WITH ALL APPROPRIATE UTILITY COMPANIES.
9. THE CONTRACTOR OR OWNER IS RESPONSIBLE FOR OBTAINING AND PAYING FOR ANY PERMITS AND/OR CONNECTION FEES REQUIRED TO PERFORM THE WORK.
10. DISPOSAL OF ALL MATERIALS IS THE RESPONSIBILITY OF THE CONTRACTOR AND MUST BE OFF-SITE IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL MUNICIPAL REQUIREMENTS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SITE RESTORATION AND CLEAN UP UPON COMPLETION OF THE PROJECT & SHALL SUBMIT AS-BUILT DRAWINGS SHOWING ALL UTILITIES AS INSTALLED. ANY REVISIONS REQUIRED BY THE CITY SHALL BE MADE AND FINAL HARD COPIES AND ELECTRONIC FILES MUST BE SUBMITTED OF THE FINAL AS-BUILT PLANS.
12. INSTALLATION WORK FOR ALL DOMESTIC WATER AND FIRE SERVICES SHALL BE PERFORMED USING WET TAPS. PLANNED DISRUPTION OF WATER SERVICE WILL NOT BE ALLOWED.
13. NO UNMETERED WATER USE DURING CONSTRUCTION IS ALLOWED. IF CONSTRUCTION WATER IS NEEDED FROM AN ADJACENT HYDRANT, THIS MUST BE COORDINATED WITH THE ASHLAND FIRE DEPARTMENT AND WATER DIVISION. A CHECKLIST FOR DEMOLITION REQUIREMENTS MUST BE OBTAINED, SIGNED AND RETURNED TO THE ENGINEERING DIVISION PRIOR TO SIGN-OFF OF ANY DEMOLITION PERMITS.
14. ANY/ALL ALTERATIONS TO UTILITIES SERVING THE PROPERTY WILL REQUIRE THAT PERMITS BE OBTAINED BY A BONDED AND INSURED CONTRACTOR THROUGH THE ENGINEERING DIVISION OF PUBLIC WORKS. THIS INCLUDES TRENCH WORK REQUIRED AND ALL WORK WITHIN THE RIGHT-OF-WAY.

DRAINAGE NOTES:

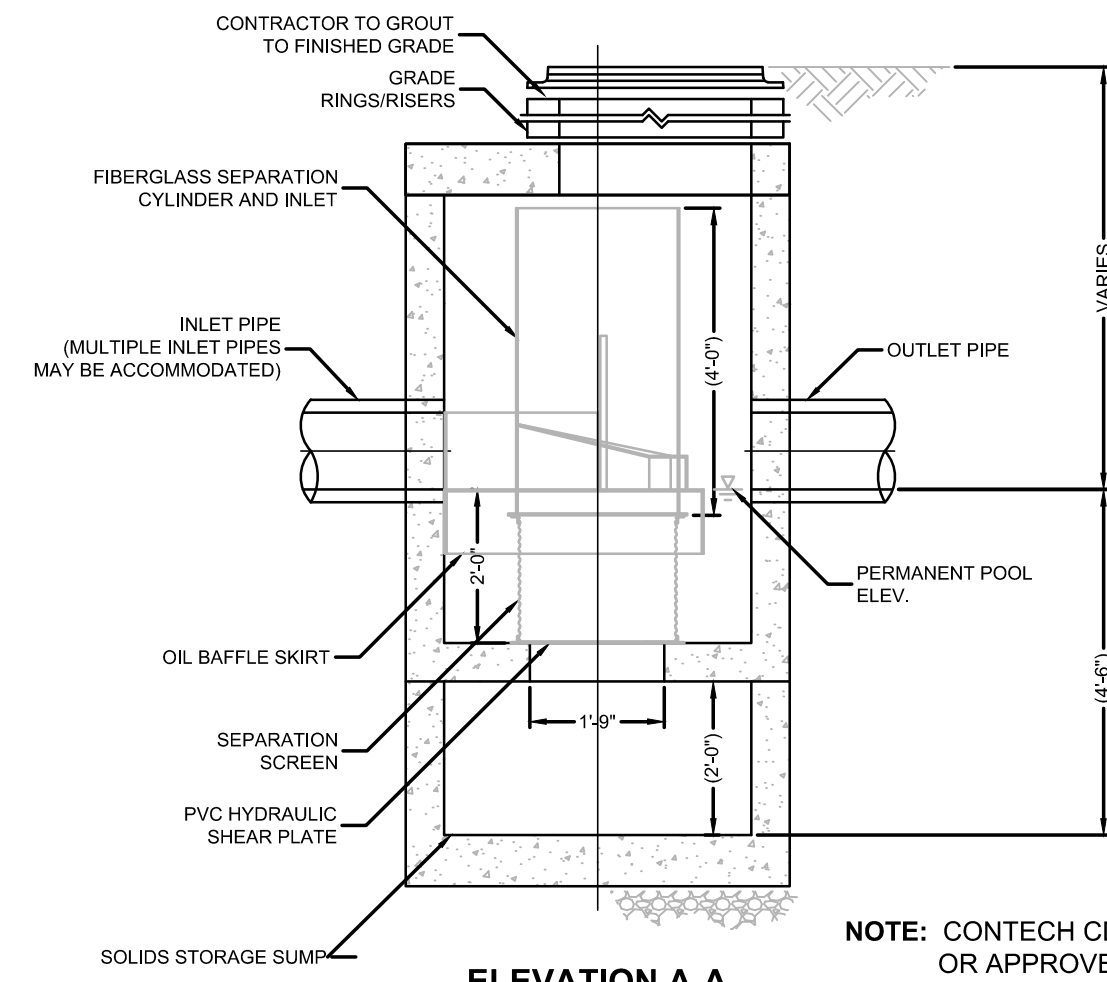
1. ALL CATCH BASINS ON-SITE SHALL BE EQUIPPED WITH GAS/OIL HOODS AND 4 FOOT SUMPS.
2. ALL STORM DRAINAGE PIPES SHALL BE 12" HIGH DENSITY POLYETHYLENE PIPE (H.D.P.E) CORRUGATED OUTSIDE & SMOOTH INSIDE UNLESS NOTED OTHERWISE.
3. A MINIMUM OF 18" VERTICAL CLEARANCE SHALL BE MAINTAINED WHERE WATER SERVICES CROSS STORM DRAIN LINES. WATER SERVICES SHALL BE ENCASED IN CONCRETE REGARDLESS OF CLEARANCE WHEN PASSING BELOW STORM DRAIN LINES. ENCASEMENT SHALL EXTEND ALONG WATER SERVICE A MINIMUM DISTANCE OF 10' FROM THE CROSSING POINT OF THE OTHER PIPE AS MEASURED NORMALLY FROM ALL POINTS ALONG THE PIPE.
4. REFER TO ARCHITECTURAL DRAWINGS FOR ALL SERVICE CONNECTIONS (I.E., ROOF DRAINS, WATER AND SEWER SERVICES, ETC.) WITHIN 10-FT OF THE BUILDING WALLS.

<p>Engineering Alliance, Inc. Civil Engineering & Land Planning Consultants 194 Central Street Saugus, MA 01906 Tel: (603) 610-7100 Fax: (603) 610-7101</p>	
<p>Proposed Site Plan 9-49 Homer Avenue (Tax Map 14 Lots 352-354) Ashland, Massachusetts</p>	<p>DATE: June 17, 2022 DWG FILE NAME: 21-58508.dwg CHECKED BY: Richard A. Salvo, P.E.</p>
<p>PROJECT: Proposed Site Plan</p>	<p>DESIGN BY: Eric Bradanes, P.E.</p>
<p>APPLICANT: 9-49 Homer Avenue, LLC 11 Plead Road Newton, MA 02459</p>	<p>DRAWING TITLE: Grading, Drainage, & Utility Plan</p>
<p>DWG. NO. C-4</p>	<p>7/28/23 10/11/22 DATE</p>
<p>MODIFIED PER PEER REVIEW COMMENTS UPDATED BUILDING FOOTPRINT DESCRIPTION OF REVISION</p>	<p>7/28/23 10/11/22 DATE</p>

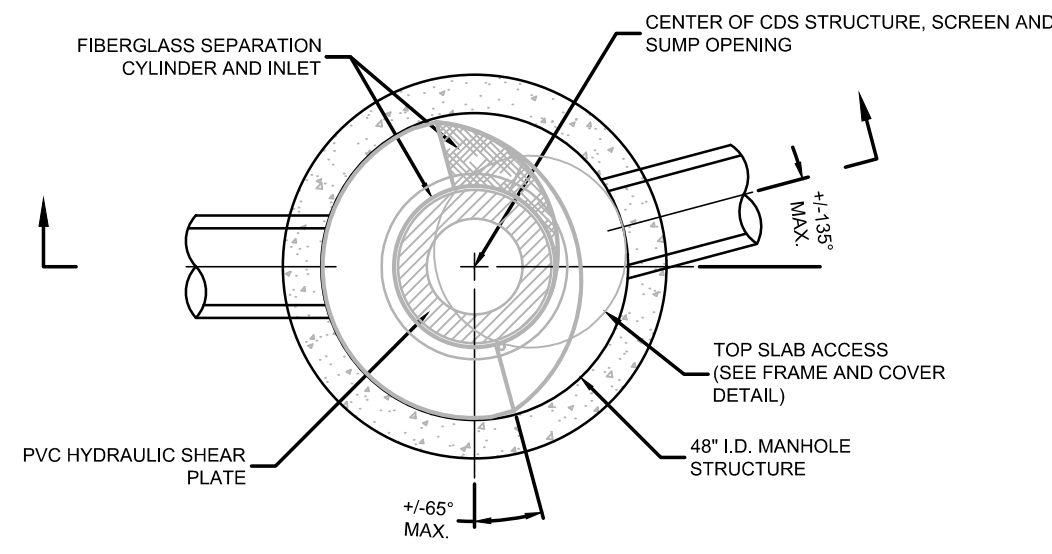


NOTES:
 1. "V" DENOTES HANDICAPPED ACCESSIBLE PARKING SPACE
 2. REFER TO SIDEWALK DETAIL FOR SIDEWALK CONSTRUCTION.

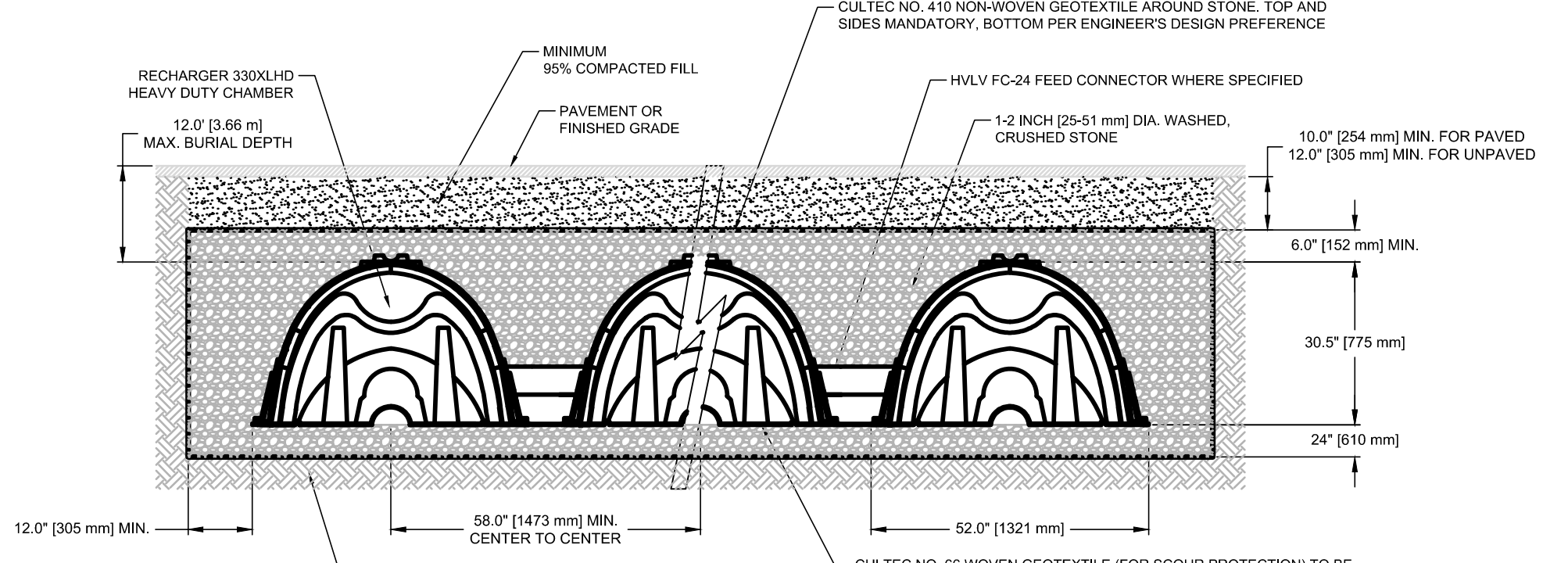
HANDICAP AND STANDARD PARKING DETAIL
 NOT TO SCALE



ELEVATION A-A
 N.T.S.
 NOTE: CONTECH CDS INLINE OR APPROVED EQUAL



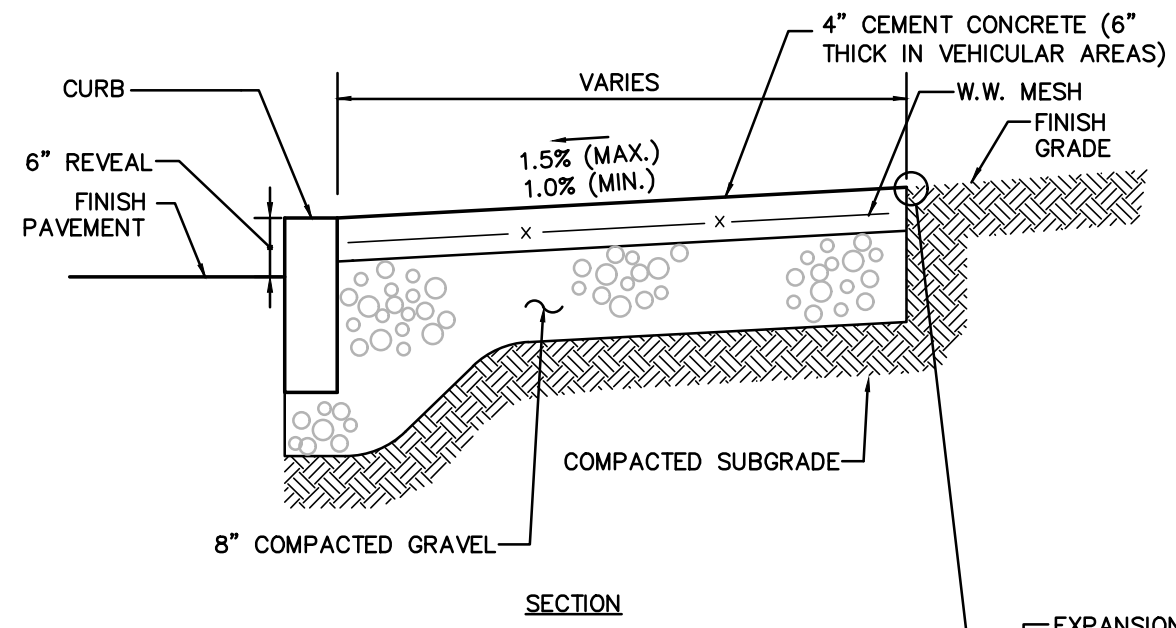
PLAN VIEW B-B
 N.T.S.



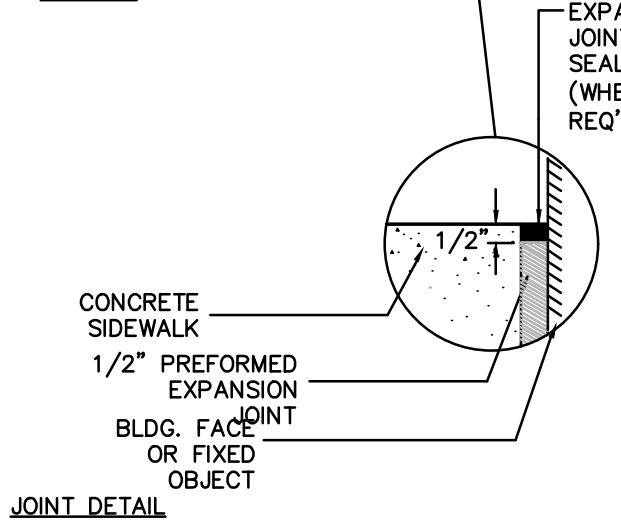
GENERAL NOTES
 RECHARGER 330XL HD BY CULTEC, INC. OF BROOKFIELD, CT. STORAGE PROVIDED = 11.32 CF/FT [1.05 m³/m] PER DESIGN UNIT. REFER TO CULTEC, INC.'S CURRENT RECOMMENDED INSTALLATION GUIDELINES. THE CHAMBER WILL BE DESIGNED TO WITHSTAND TRAFFIC LOADS WHEN INSTALLED ACCORDING TO CULTEC'S RECOMMENDED INSTALLATION INSTRUCTIONS.

ALL RECHARGER 330XL HD HEAVY DUTY UNITS ARE MARKED WITH A COLOR STRIPE FORMED INTO THE PART ALONG THE LENGTH OF THE CHAMBER. ALL RECHARGER 330XL HD CHAMBERS MUST BE INSTALLED IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.

TYPICAL CROSS SECTION

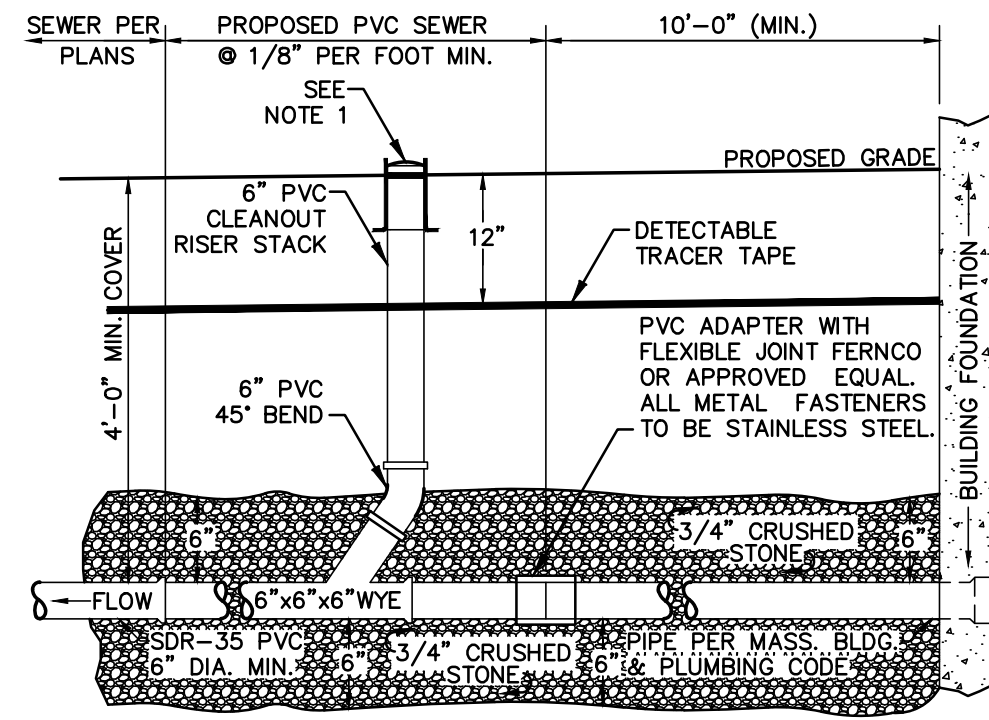


NOTES:
 1. PROVIDE EXPANSION JOINTS AT MIN. 30 FT. O.C. WITH PRE-MOLDED JOINT FILLER.
 2. PROVIDE CONTROL JOINTS AT 6' O.C.
 3. PROVIDE BROOM FINISH IN DIRECTION PERPENDICULAR TO CURB.
 4. CEMENT CONCRETE SHALL BE 4000 PSI-TYPE II



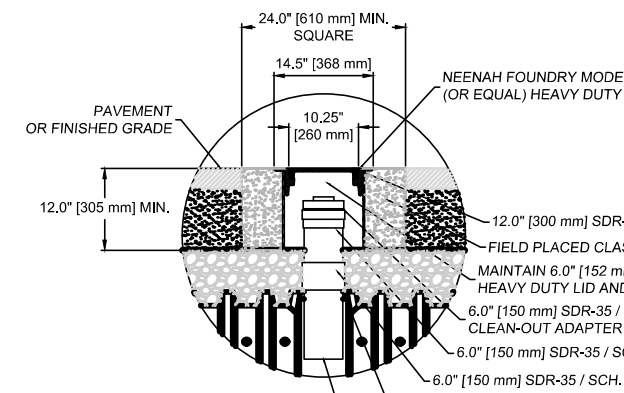
CONCRETE SIDEWALK
 NOT TO SCALE

CONTECH CDS WATER QUALITY MANHOLE
 NOT TO SCALE

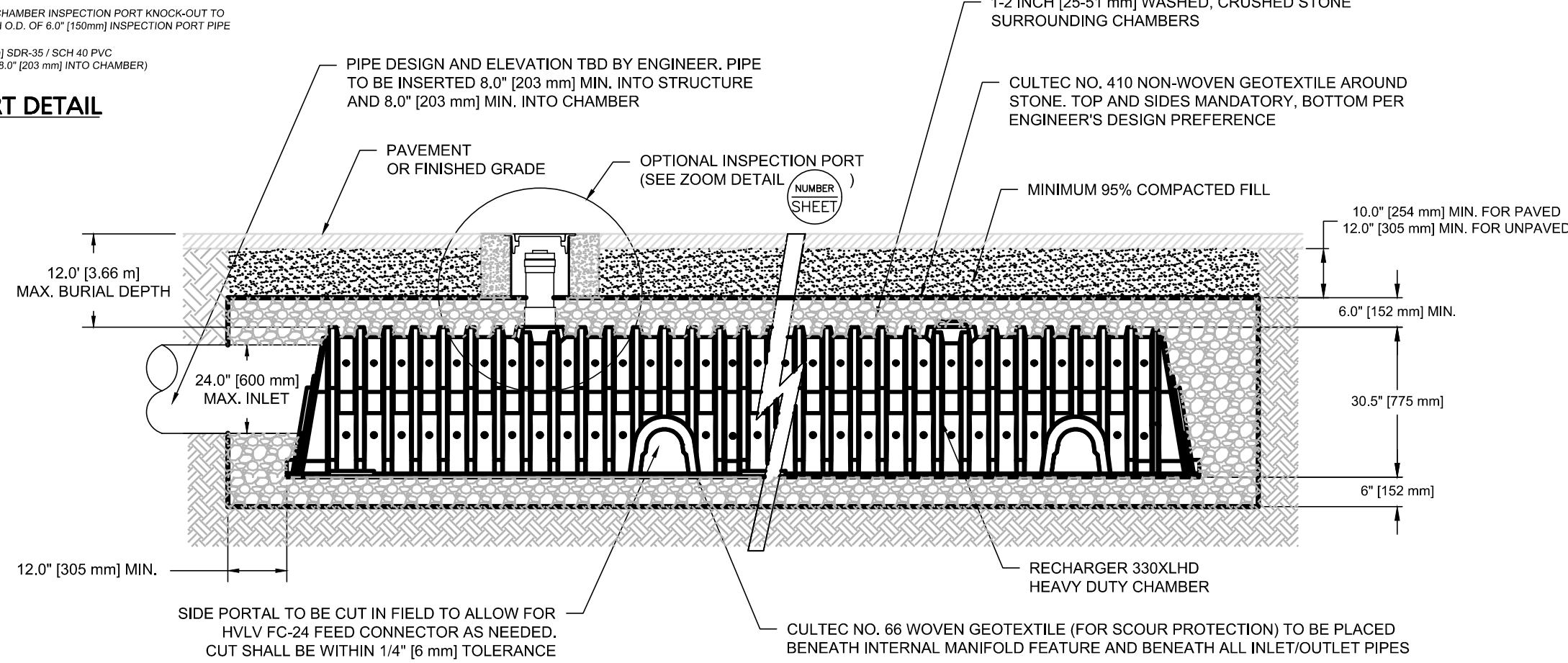


NOTES:
 1. LeBARON CAST IRON SEWER CLEANOUT RING COVER. COVER TO BE CAST WITH THE WORD "CLEANOUT" AND HAVE FLAT DIAMOND SURFACE.

SANITARY SEWER SERVICE AT BUILDING
 NOT TO SCALE

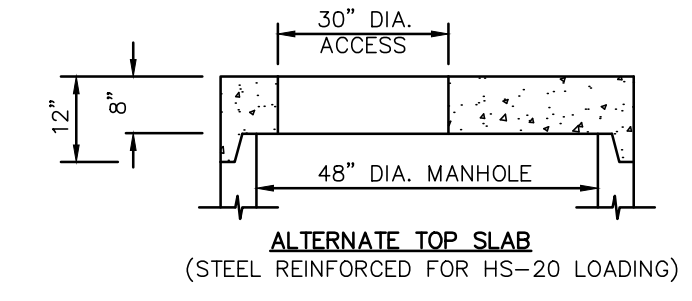


INSPECTION PORT DETAIL

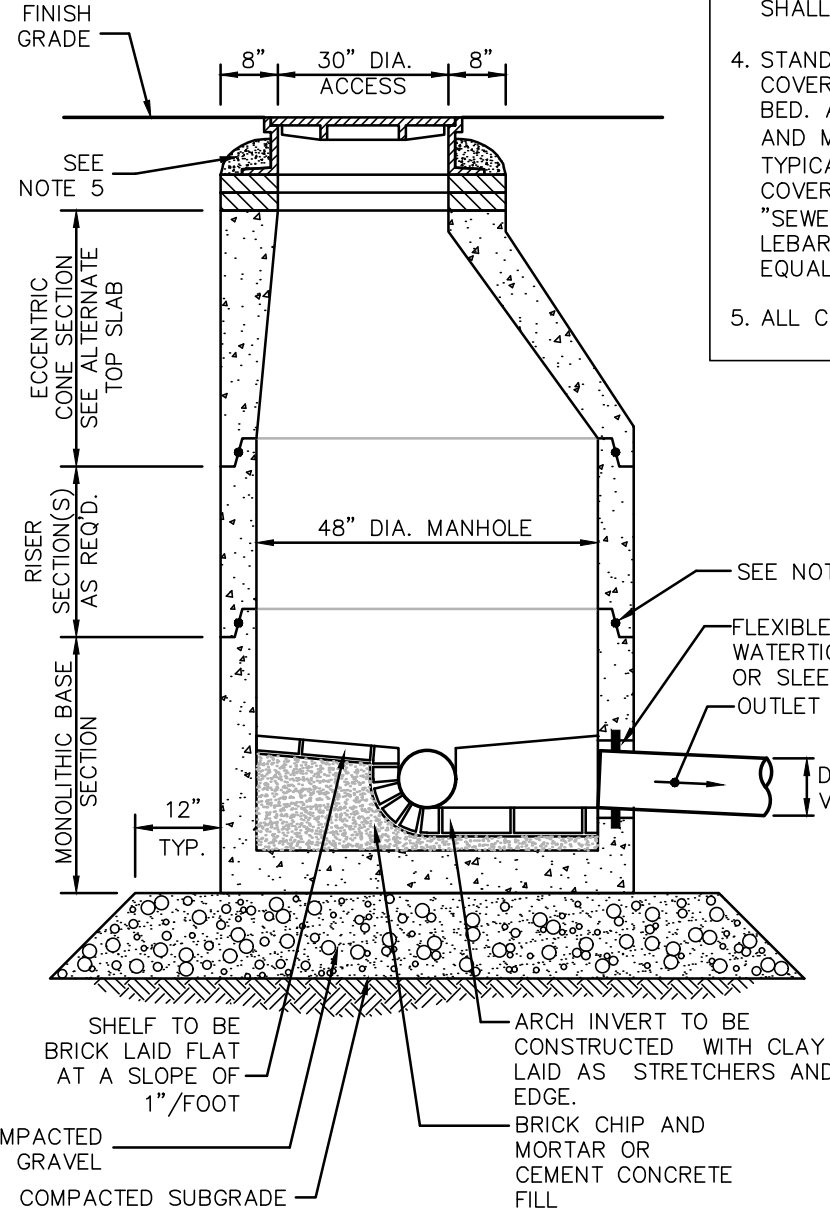


MAINFOLD DETAIL
SUB-SURFACE DRAINAGE FACILITY DETAIL
CULTEC 330XL HD
 NOT TO SCALE

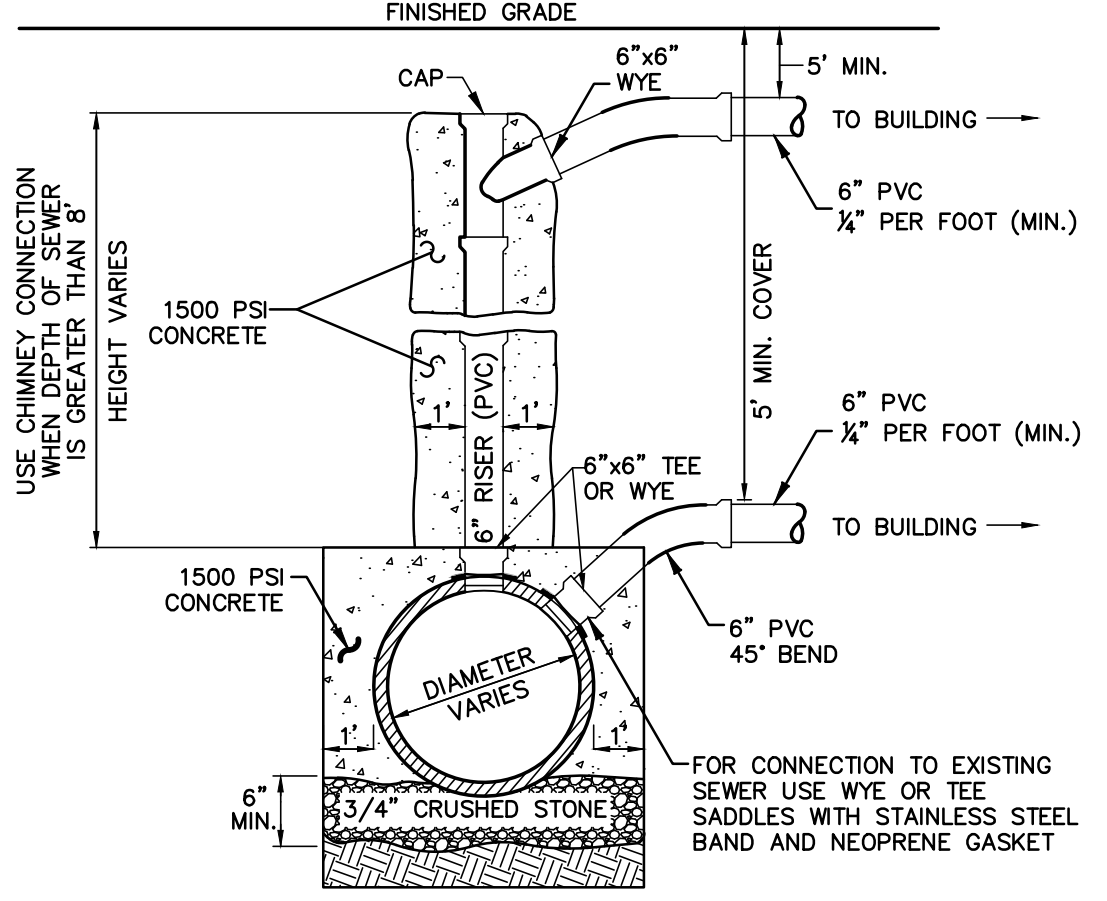
NOTES:
 1. ALL SECTIONS SHALL BE DESIGNED FOR HS-20 LOADING.
 2. COPOLYMER MANHOLE STEPS SHALL BE INSTALLED AT 12" O.C. FOR THE FULL DEPTH OF THE STRUCTURE.
 3. PROVIDE "V" KNOCKOUTS FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE. MORTAR ALL PIPE CONNECTIONS.
 4. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE PERFORMED BUTYL RUBBER.
 5. DRAIN MANHOLE FRAME AND COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM)



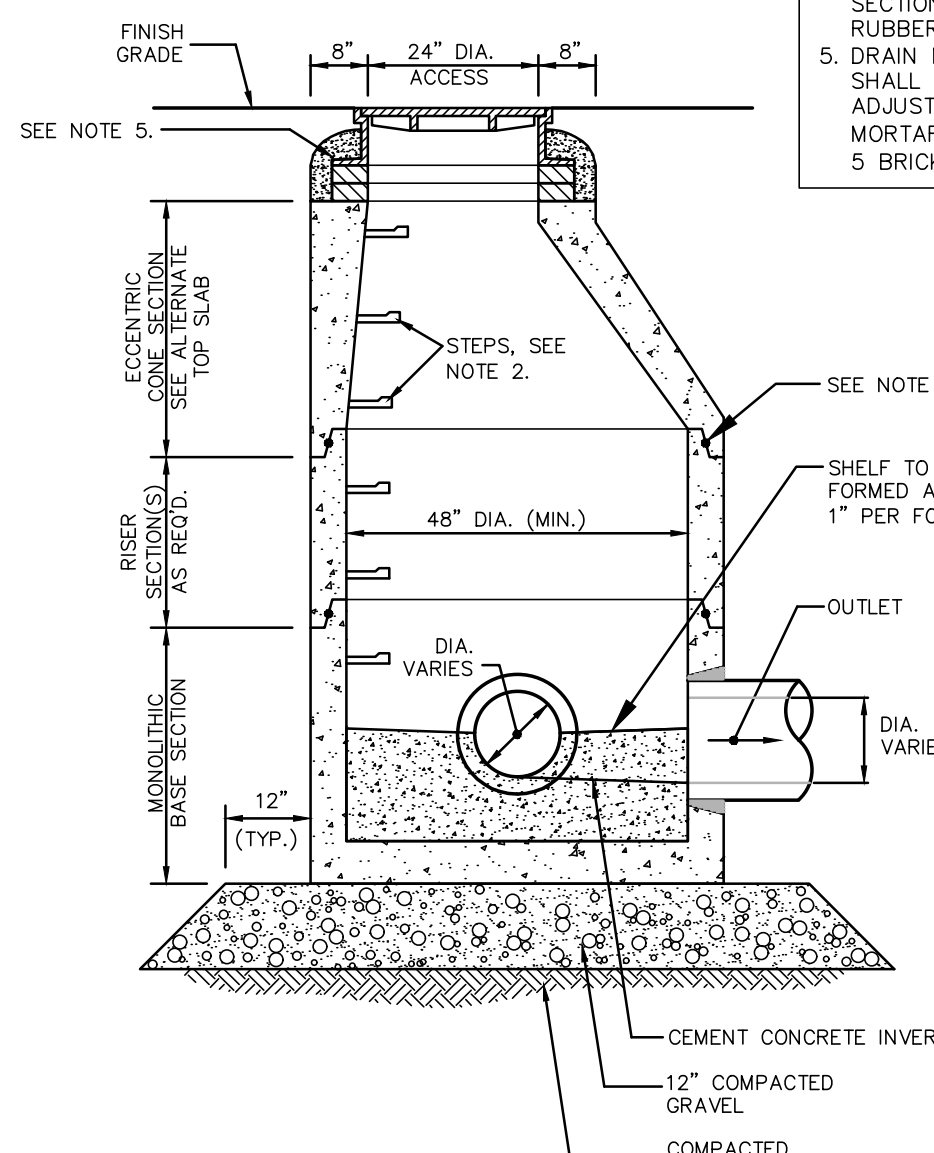
NOTES:
 1. ALL SECTIONS SHALL BE DESIGNED FOR HS-20 LOADING.
 2. ALL EXTERIOR SURFACES SHALL BE GIVEN TWO COATS OF BITUMINOUS WATER-PROOFING MATERIAL.
 3. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE NPC FLEXIRIB OR EQUAL.
 4. STANDARD SEWER MANHOLE FRAME AND COVER SHALL BE SET IN FULL MORTAR BED. ADJUST TO GRADE WITH CLAY BRICK AND MORTAR (2 BRICK COURSES TYPICALLY, 5 BRICK COURSES MAXIMUM). COVER SHALL BE LABELED WITH THE WORD "SEWER" IN 3-INCH LETTERS AND BE LEBARON FOUNDRY MODEL LA246 OR EQUAL.
 5. ALL CONCRETE SHALL BE 5,000 PSI



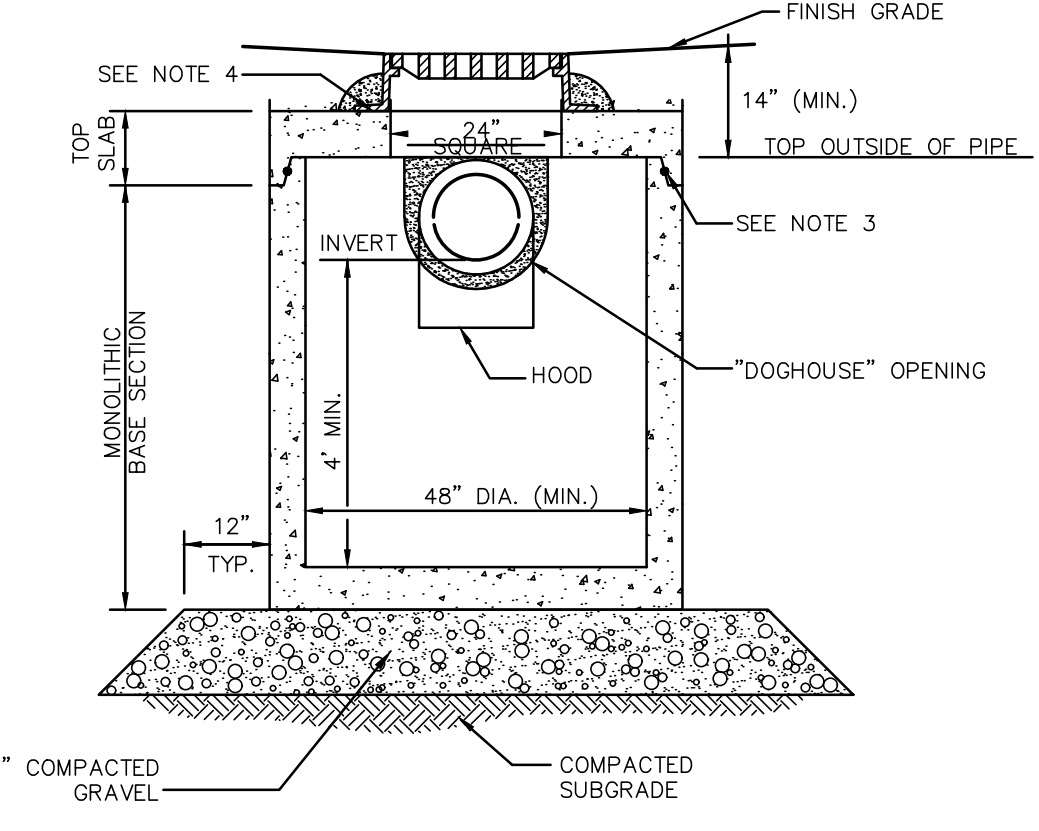
SANITARY SEWER MANHOLE
 NOT TO SCALE



SANITARY SEWER SERVICE AT MAIN
 NOT TO SCALE



TYPICAL DRAIN MANHOLE
 NOT TO SCALE



TYPICAL CATCH BASIN SHALLOW COVER WITH HOOD
 NOT TO SCALE

NOTES:
 1. ALL SECTIONS SHALL BE DESIGNED FOR HS-20 LOADING.
 2. PROVIDE DOGHOUSE OPENING FOR PIPES WITH 2" MAX. CLEARANCE TO OUTSIDE OF PIPE. TOP SLAB SHALL NOT REST DIRECTLY ON PIPE. GROUT ALL PIPE CONNECTIONS (NON-SHRINK GROUT).
 3. JOINT SEALANT BETWEEN PRECAST SECTIONS SHALL BE PERFORMED BUTYL RUBBER.
 4. CATCH BASIN FRAME AND GRATE (4" DEPTH) SHALL BE SET IN FULL MORTAR BED.
 5. ADJUST TO FINISH GRADE WITH CLAY BRICK AND MORTAR AS REQUIRED.

Engineering Alliance, Inc.
 Civil Engineering & Land Planning Consultants
 194 Central Street
 Portsmouth, NH 03801
 Tel: (603) 610-7100
 Fax: (603) 610-0020

Proposed Site Plan
 9-49 Homer Avenue
 (Tax Map 14 Lots 352-354)
 Ashland, Massachusetts

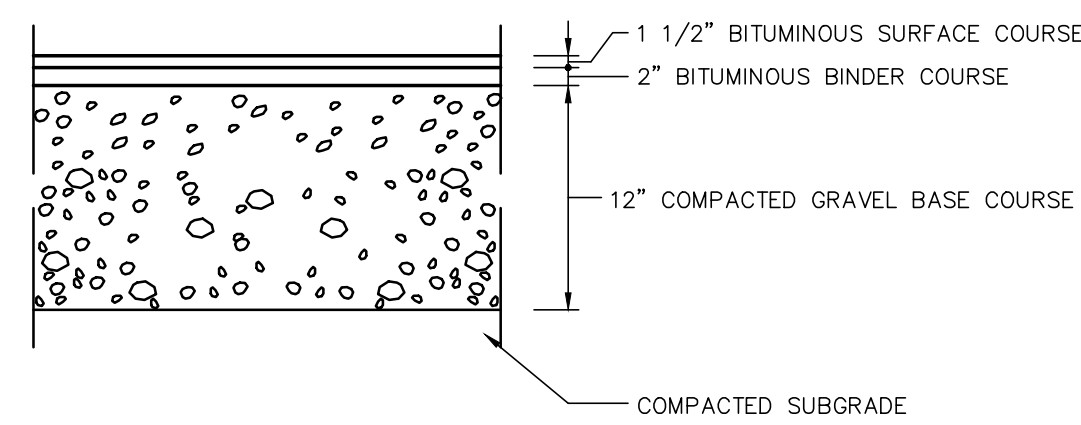
DATE: June 17, 2022
 DWG FILE NAME: 21-58508.dwg
 SCALE: AS NOTED
 CHECKED BY: Richard A. Salvo, P.E.

PROJECT: Proposed Site Plan
 PROJECT # : 21-58508
 SCALE: AS NOTED
 DESIGN BY: Eric Bradanese, P.E.

APPLICANT: 9-49 Homer Avenue, LLC
 11 Piccad Road
 Newton, MA 02459

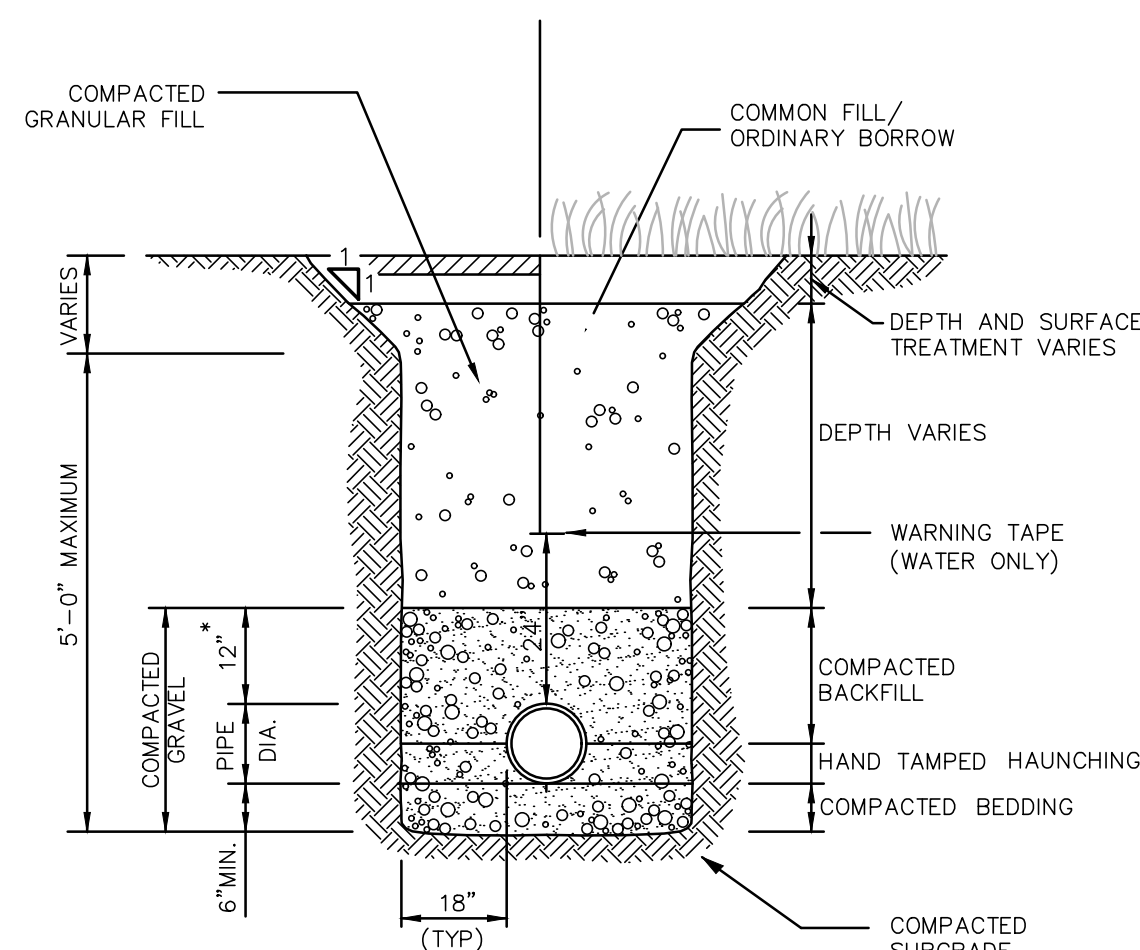
DRAWING TITLE: Construction Details
 DWG. NO. D-1

7/28/23 DATE
 MODIFIED PER PER REVIEW COMMENTS
 DESCRIPTION OF REVISION



STANDARD DUTY FLEXIBLE PAVEMENT

BITUMINOUS CONCRETE PAVEMENT SECTION
NOT TO SCALE



UTILITY TRENCH
NOT TO SCALE

TABLE OF DIMENSIONS

BENDS	B	C	D	E	F	BENDS	B	C	D	E	F	
6"	11 1/4"	8"	15"	12"	24"	12"	6"	45"	8"	30"	12"	24"
6"	22 1/2"	-	19"	-	13"	6"	90"	-	30"	-	27"	
8"	11 1/4"	-	20"	-	12"	8"	45"	-	30"	-	24"	
8"	22 1/2"	-	22"	-	17"	8"	90"	-	38"	-	36"	
12"	11 1/4"	-	30"	-	15"	12"	45"	-	40"	-	40"	
12"	22 1/2"	-	35"	-	25"	12"	90"	-	60"	-	52"	

BENDS

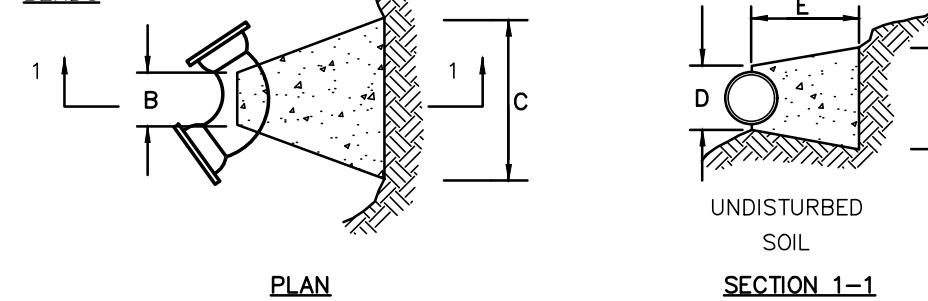
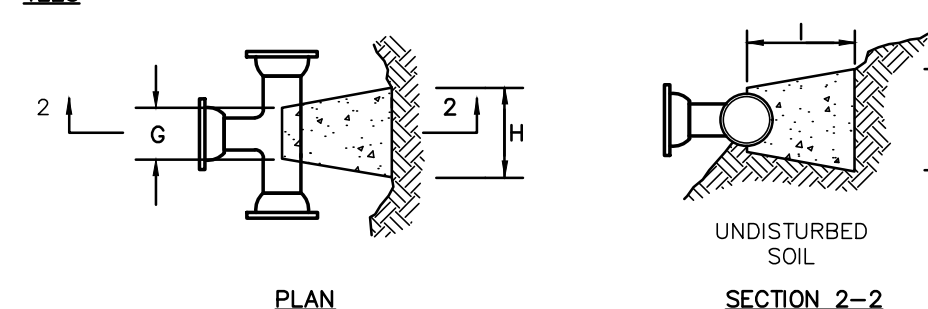


TABLE OF DIMENSIONS

TEES	G	H	I	J	TEES	G	H	I	J
6" x 6" x 6"	12"	24"	24"	18"	12" x 12" x 6"	12"	24"	24"	18"
8" x 8" x 6"	-	-	-	-	12" x 12" x 8"	-	-	-	24"
8" x 8" x 8"	-	-	-	-	12" x 12" x 12"	-	-	-	36"

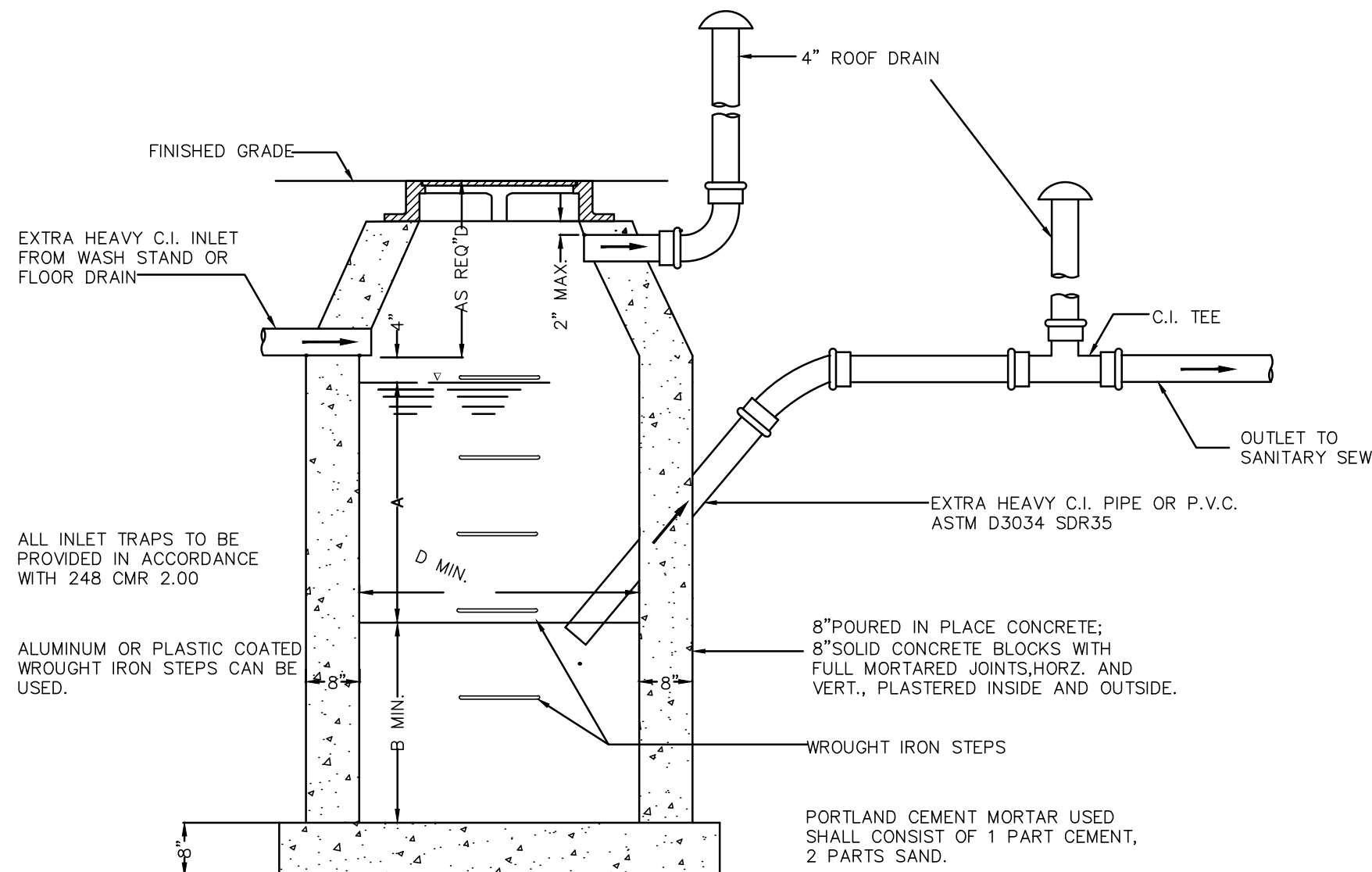
TEES



- NOTES:**
1. PROVIDE BLOCKS FOR TAPPING SLEEVES, DEAD ENDS, GATE VALVES, AND VERTICAL BENDS (SAME SIZE AS REQUIRED FOR TEES). PROVIDE ANCHOR RODS AT VERTICAL BENDS AND GATE VALVES.
 2. CONCRETE SHALL NOT BE PLACED AGAINST PIPE BEYOND FITTING.
 3. CONCRETE SHALL BE 3000 PSI-TYPE I.

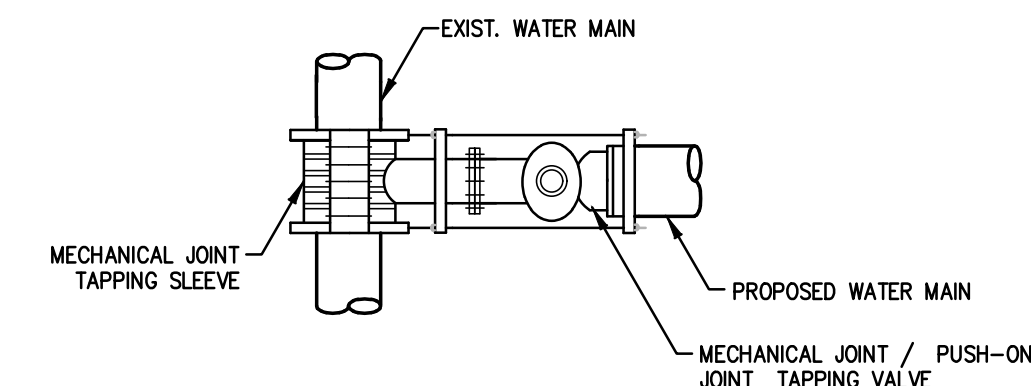
CONCRETE THRUST BLOCK
NOT TO SCALE

INLET	D	A	B	INLET	D	A	B
4"	3'-6"ø	3'-0"	2'-6"	8"	5'-0"ø	6'-0"	5'-0"
5"	3'-6"ø	5'-0"	4'-0"	5'-6" x 5'-6"	4'-6"	4'-0"	4'-0"
	3'-6" x 3'-6"	4'-0"	3'-0"	6'-0"ø	4'-0"	3'-6"	3'-6"
	3'-6" x 3'-6"	3'-6"	3'-0"	6'-0" x 6'-0"	3'-0"	2'-6"	2'-6"
	3'-6" x 3'-6"	3'-0"	2'-6"	6'-6"ø	3'-6"	3'-6"	3'-6"
	3'-6" x 3'-6"	3'-0"	2'-6"	6'-6" x 6'-6"	3'-0"	2'-6"	2'-6"
6"	4'-0"ø	5'-0"	4'-6"	10"	5'-6"ø	7'-6"	6'-6"
	4'-0" x 4'-0"	4'-0"	3'-6"	6'-0" x 6'-0"	5'-6"	4'-6"	4'-6"
	4'-6"ø	4'-0"	3'-6"	6'-0"ø	5'-6"	4'-6"	4'-6"
	4'-6" x 4'-6"	3'-6"	3'-0"	6'-0"ø	6'-6"	5'-6"	5'-6"
	5'-0"ø	3'-6"	3'-0"	6'-6" x 6'-6"	5'-0"	4'-0"	4'-0"
	5'-0" x 5'-0"	3'-0"	2'-6"				

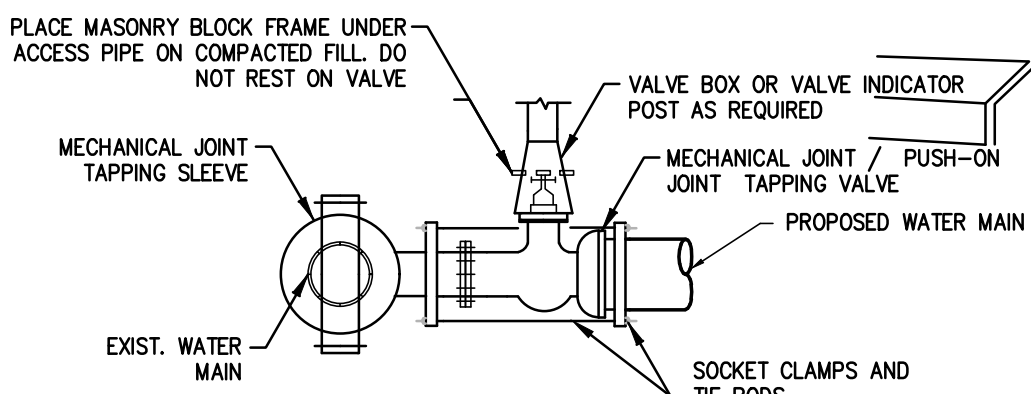


- NOTES:**
- ALL INLET TRAPS TO BE PROVIDED IN ACCORDANCE WITH 248 CMR 2.00.
 - ALUMINUM OR PLASTIC COATED WROUGHT IRON STEPS CAN BE USED.
 - FOR USE TO DRAIN INDOOR COVER AREAS ONLY.
 - FOR INLETS LARGER THAN 10" THE DESIGN AND DIMENSIONS WILL BE DETERMINED FOR EACH PARTICULAR CASE.
 - PRE-CAST SEPARATORS ARE TO HAVE ALL SPECIFIED HOLES EITHER CORE-BORED OR CAST IN PLACE.

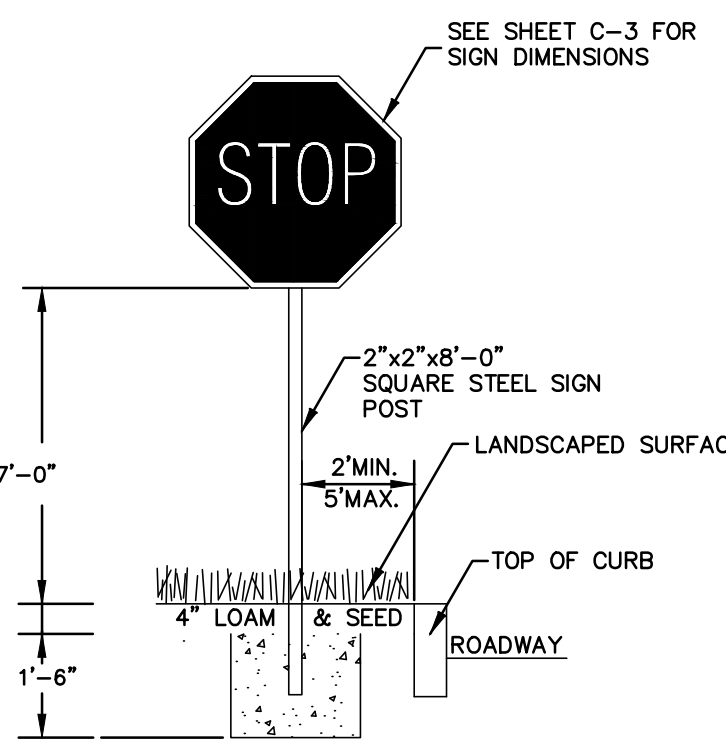
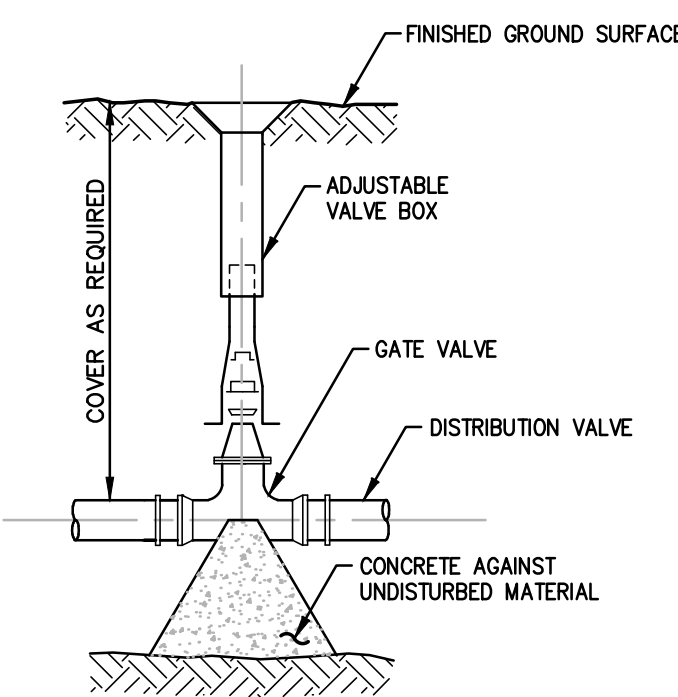
GAS TRAP
SCALE: N.T.S.



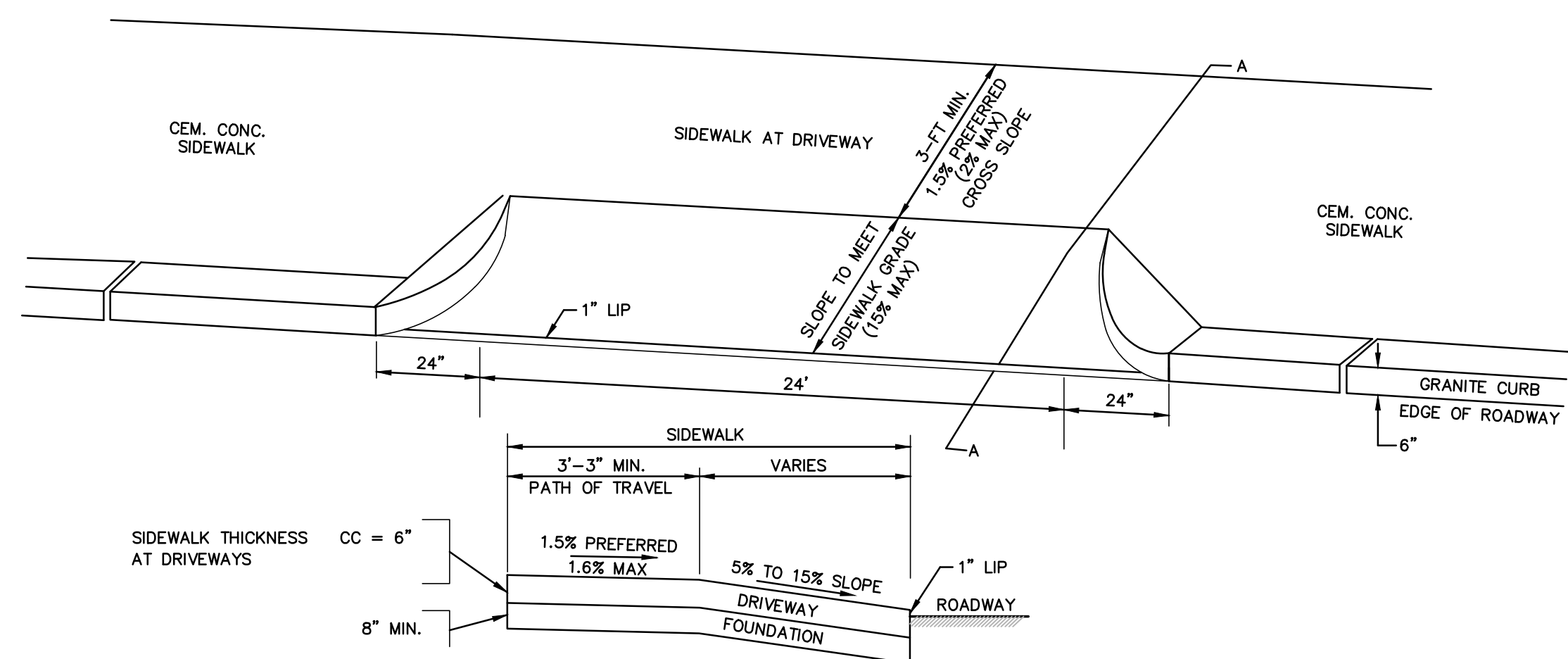
TAPPING SLEEVE AND VALVE
NOT TO SCALE



WATER GATE DETAIL
NOT TO SCALE



TYPICAL SIGN DETAIL
NOT TO SCALE



SECTION A-A DRIVEWAY DETAIL
NOT TO SCALE

DATE	DESCRIPTION OF REVISION
7/28/23	MODIFIED PER PEER REVIEW COMMENTS

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SCALE: AS NOTED
DESIGN BY: Eric Bradshaw, P.E.
CHECKED BY: Richard A. Salvo, P.E.

Professional Engineer for
Engineering Alliance, Inc.

APPLICANT: **9-49 Homer Avenue, LLC**
11 Picked Road
Newton, MA 02459

DRAWING TITLE: **Construction Details**

DWG. NO. **D-2**